

# The Neighborhood at Indian Creek



*Design Guidelines Manual*  
December 2009, Revised March 2019





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## ACKNOWLEDGEMENTS

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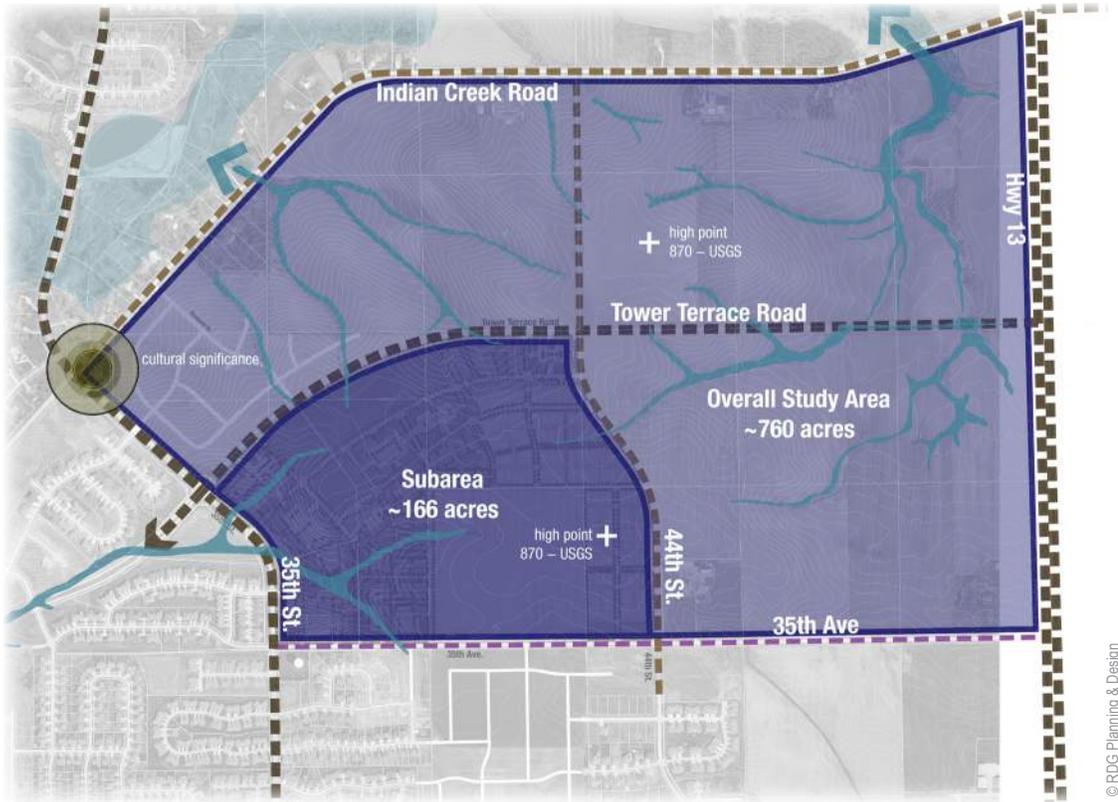
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The following design guidelines are a series of recommendations for the flexible, functional, cohesive vision for The Neighborhood at Indian Creek. These guidelines are intended to be used by the City of Marion as a foundation for this development.



**FIGURE 1: EXISTING CONDITIONS DIAGRAMMING**

This diagram illustrates the site's topography and drainage patterns within its overall context. The Master Plan for the area is informed by the existing landform of the site and seeks to minimize the physical impact of the area's development.



**FIGURE 2: EXISTING PROPERTY OWNERSHIP**

This diagram reflects the current property ownership for the lands underlying the master plan of The Neighborhood at Indian Creek study area.

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FIGURE 3: PROJECT LOCATION



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FIGURE 4: THE NEIGHBORHOOD AT INDIAN CREEK MASTER PLAN  
The illustrative plan above is a diagrammatic representation of the vision for the development.

## DEVELOPMENT INTENT

The Neighborhood at Indian Creek is planned within a rapidly growing eastern Iowa metropolitan area. Situated within the City of Marion and within an easy commute to the larger metropolitan areas of Cedar Rapids and Waterloo, The Neighborhood will meet the residential and mixed use commercial needs of a wide cross-section of the market-place.

The emerging vision for The Neighborhood at Indian Creek is a mixed use development with midwestern architectural character and a neighborhood feel developed through neo-traditional planning. The plan recommends the integration of a large centrally-located community park and a smaller commercial center with a green space square at the heart of the area. The larger park provides gathering space for passive recreation and community events and the opportunity for neighbors and greater Marion to interact regularly. The square, located within

proximity to the park and major thoroughfares, also works to create a sense of community by placing a public gathering and events space at the center of The Neighborhood's commercial services area where day-to-day necessities can be met.

A primary feature of The Neighborhood is its walkability. The pedestrian takes precedence in this community. Uninterrupted walkways can be found on most streets as the driveways and typical residential services (trash collection, utility maintenance, vehicular ingress and egress) are located at the back of houses. Community open space, pedestrian corridors, greenways, limited travel distances and centrally located amenities all encourage neighbors to travel by foot and to interact daily.



### DESIGN CHARACTER IMAGERY:

The images above represent aesthetic characteristics that are proposed to permeate The Neighborhood at Indian Creek.

## PROJECT OVERVIEW

The Neighborhood at Indian Creek is a uniquely planned mixed use development within the rapidly expanding community of Marion, Iowa. The following design guidelines are intended to provide an armature and foundation for future community development and seek to create a neighborhood environment where residents develop a sense of belonging and connection to the community. The Neighborhood reflects the needs of Marion and the people it belongs to.

It is the intent of this planning effort to establish the framework for a pedestrian oriented and aesthetically dynamic new neighborhood for the City of Marion.

In the spring of 2009, the City of Marion retained RDG Planning & Design (RDG) and its multi-disciplinary design team to engage the community in an integrated and interactive design process that would lead the community to a master plan for approximately 150 acres of the 29th Avenue North Subarea. This design process utilized the existing study information and added a level of detail necessary to move from land use to neighborhood development. The design team engaged local community leaders, property owners and City Staff in a day-long design work session and multiple small and large group design review meetings to gain knowledge and understanding about the history and present and potential future of the project area. These meetings were supplemented by small group precedent tours where the design team guided members of the Marion community through many existing developments in the Des Moines metro area with similar characteristics to the plan being proposed. This interactive design process began in April of 2009 with meetings regularly occurring through August 2009. Through these sessions, design concepts for The Neighborhood were created. These concepts were reviewed by the groups for strengths, weaknesses, and suitability to the community as they were developed. Through this iterative process, the group conveyed their local knowledge and insight to the design team and a consensus-based master plan for the successful development of the project was distilled.

In 2017, the City of Marion reengaged RDG to update the plan based on current conditions of the area. In addition to a master plan update, RDG created district standards that guide the development character.

Significant elements of the plan are a neighborhood square, a community park and context-based residential and commercial architecture combined with the integration of alleyways for the rear loading of dwellings. Minimized residential setbacks make the fronts of homes within The Neighborhood more conducive to community interaction. Each of these elements is directly influ-

enced by the uniqueness of the community and informed by the ongoing discussions about what makes an ideal neighborhood for Marion.

### NEIGHBORHOOD SQUARE

The neighborhood square will serve the community as a space intended to be a flexible and functional public facility. A platform for local artists and gathering place for meetings and events, the neighborhood square will lend itself to a range of uses within The Neighborhood.

### COMMUNITY PARK

The community park is intended to be a park reserved for the larger community. This park is envisioned as an oasis of passive recreation within the Marion Parks System, not intended for organized sport activities, and a central park for The Neighborhood.

### COMMERCIAL ARCHITECTURE

The architecture of commercial structures within The Neighborhood will be based in the contextual form and pattern of a turn of the century Midwestern small town similar to Uptown Marion with contemporary interpretation of forms and material updates. It is the desire of this planning effort that these buildings utilize local, natural materials such as stone and brick masonry as well as wood for façade applications.

### RESIDENTIAL ARCHITECTURE

Architecture within the residential areas of The Neighborhood are intended to be an eclectic mix of styles from the broader community. These styles will then be enhanced by the use of color throughout. It is the desire of this planning effort to promote a diversity of residential architecture and to minimize the further construction of monolithic developments of housing with only one style of housing type and the limited use of neutral color schemes.

### FUTURE SCHOOLS

The Linn-Mar Community School District owns land within the Neighborhood and has plans for two future schools on the site. The first school is planned to be an intermediate building and is scheduled to open for the 2020-2021 school year. The second school is planned to be an elementary school.

### MARKET CONSIDERATIONS

Under the original 2009 Plan for The Neighborhood, land uses and their boundaries within the project area were defined by combining a Market Assessment and Analysis with the current housing and structures in place within the community. While economic and housing conditions have changed since the 2009 market analysis, Marion continues to grow at a high rate. Development in the area is eminent and interest in the area is high.

The 2019 Update does not rely on the previous market analysis but on development trends, external development pressures such as the new Linn-Mar School site, community interests, and changing property ownership. The following design guidelines are the tangible expression of the research performed, multiple interviews conducted, stakeholder input sessions held, and consensus realized.



### ZONING DISTRICTS

To implement the vision and development intent for The Neighborhood at Indian Creek, this document lays out a Planned Development District with framework to apply to the area when development proposals come forward. The Neighborhood Planned Development District allows for a variety of housing options and compatible neighborhood commercial and personal services in a concentrated area that aligns with creating a pedestrian focus. The Neighborhood Planned Development District includes three distinct districts; district standards are summarized in the appendix and detailed throughout this document. The community desires The Neighborhood to be unique to Marion; as such, the district standards are also unique. Standards focus on form and function over land use type. Density is allowed and encouraged while prominent surface parking and significant impervious areas are discouraged.

### ZONING DISTRICT STANDARDS

The character of the neighborhood transitions between commercial and residential uses to function as a cohesive neighborhood unit. Therefore, the plan proposes three districts for development application. Note that the figure shown below is a representation of the proposed zoning framework. The actual boundaries may vary slightly as development proposals are heard and evaluated.

### NEIGHBORHOOD MIXED USE (MX)

The Neighborhood Mixed Use district (MX) encompasses the areas within the Neighborhood at Indian Creek Master Plan located east of 35th Street, north of 35th Avenue, south of Tower Terrace Road and west of a to be determined street/

avenue next to the community park. The MX district is an area where commercial services, retail uses, and residential uses are encouraged to develop in a horizontally and vertically mixed-use environment. Higher densities and intensities will be provided for and are allowed in this area. The intent is to realize a mixed use development with midwestern architectural character that functions as a center for the surrounding neighborhood(s).

### MULTI-UNIT RESIDENTIAL (MUR)

The Multi-Unit Residential district (MUR) encompasses the areas within The Neighborhood at Indian Creek Master Plan that generally abut the MX district to act as a buffer between the UR district. The MUR District is an area where high density residential is encouraged such as apartments, condominiums, and terrace communities. Commercial uses may be allowed on ground floors at intersections. The intent is to realize a traditional neighborhood development complemented by community commercial and civic uses.

### URBAN RESIDENTIAL (UR)

The Urban Residential district (UR) encompasses the areas within The Neighborhood at Indian Creek Master Plan located generally east of Graceland Boulevard, north of 35th Avenue (excluding the school), south of Tower Terrace Road, and west of 44th Street. The UR district is an area where a mix of low to medium density residential uses are encouraged such as single-family dwellings, duplexes, and townhomes. The intent is to realize a traditional neighborhood development complemented by community commercial and civic uses. The style of the residential uses should provide a cohesive look and feel to the district.



FIGURE 5: RECOMMENDED DISTRICT APPLICATION

The plan above is a diagrammatic representation of the recommended district land uses.



**FIGURE 6: OPEN SPACE, PARKLAND, BICYCLE & PEDESTRIAN CONNECTIONS**

The above figure illustrates the incorporation of bicycle/pedestrian connections and open space into The Neighborhood at Indian Creek. The community park and the neighborhood square would be utilized as passive and active recreation space within the development. The other light green highlighted areas are the general locations reserved for stormwater detention (beyond small detention possible within the community park) and possible passive recreation space. The green dashed lines show the proposed location of bicycle lanes or sidepaths; the yellow dashed lines represent pedestrian paths. All streets should also serve as pedestrian-friendly routes to the community park, the neighborhood square, and school site.



**FIGURE 7: COMMUNITY PARK & NEIGHBORHOOD SQUARE ENLARGEMENT**

This illustration represents a dynamic vision for a community park and neighborhood square.

## PARKLAND AND OPEN SPACE

### INTENT

Developers are recommended to dedicate land for public use as a method of ensuring that new development provides for a portion of the park needs created by the large number of new residents, workers and their families moving into the new neighborhood.

The community park, located along and south of Tower Terrace Road, and the neighborhood square, located within The Neighborhood Mixed Use district, are potential areas for dedicated parkland within the development (see Figure 6 for locations).

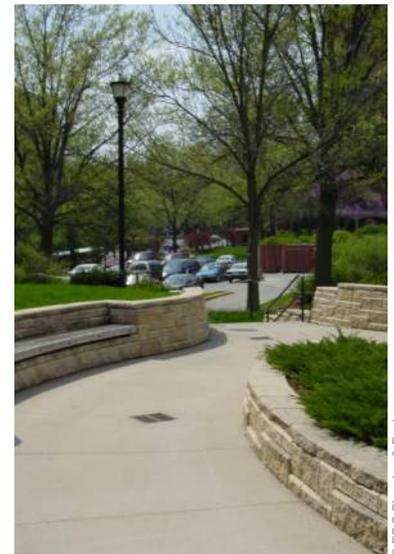
### COMMUNITY PARK

This passive recreational community park, unlike the neighborhood square, will receive less scheduled programming and will be able to facilitate larger gatherings and performances as necessary. The park will likely entail public walkways, indigenous landscapes for both aesthetic and stormwater quantity mitigation and quality enhancement purposes, public shelters and many other amenities that might typically be found within a community park. The Community Park will be surrounded on three sides (west, south and east) by commercial or residential architecture of a scale that will allow it to act as an urban edge for the park. To its north, the park will be bordered by Tower Terrace Road. This roadway will allow the connection of community trails into the park system and will assist citizens of both The Neighborhood and Marion to move from this area into the larger community and vice versa.

### NEIGHBORHOOD SQUARE

The neighborhood square is intended to be a flexible and functional public space. The square itself will allow for multiple types and sizes of public gatherings for several purposes, will act as a community meeting place when needed, and will provide a venue for local performing and visual artists. The square will be bordered on three sides (west, south, and east) by on-street parking and commercial space. The scale of these structures should be such that they will create an urban edge to the square, conveying the feel of an outdoor room for those who use the space.

It is recommended that the City of Marion develop a parkland dedication policy and accompanying ordinance. Specifically in this area, the City and the owner(s) should complete a Development Agreement detailing the specifics relating to the dedication of parkland denoted in the plan. Ideally, the City and property owner would develop an agreement regarding the purchase of the parkland identified, and the property would be platted and dedicated at such time the areas is developed. A per acre fee would be established for the park's service area which would be paid to the City upon the final platting of the property. This would appropriately compensate the owner; provide a mechanism for the City to recoup its cost, and provide a neighborhood park to serve the area. Preferably, the identified Development Agreement would be tailored to a specific development plan as a part of the creation of a Planned Development District. Regardless, to assure that adequate land is preserved for parks, the city and owner should pursue the completion of a Development Agreement upon adoption of the Plan.



### PARK CHARACTER IMAGERY

The images above are representative of aspects of the park land open space envisioned within The Neighborhood.

## STORMWATER MANAGEMENT

### INTENT

It is the recommendation of this plan that all aspects of the development of The Neighborhood utilize Best Management Practices for stormwater management. “Best Management Practice” (BMP) is a blanket term used to describe a management practice or technique that has been deemed to be effective and practical in mitigating stormwater run-off from a developed site.

A combination of stormwater BMPs that are integrated into site development zones and are designed to filter and infiltrate runoff before being discharged into the natural drainage system are possible within the development framework shown in this plan. While a comprehensive systems approach is not detailed in this plan a next step would be the design of a system to manage the quality and quantity of stormwater by increasing runoff retention time on site, infiltrating water into the ground and plant roots, filtering pollutants and maintaining or reducing the overall runoff volume leaving the site. Stormwater BMPs that could contribute to this system through integration into development zones could include a combination of the following:

- Reduced street/drive widths and paved areas to minimize impervious cover and stormwater runoff rate and volume.
- Green roofs on built structures to absorb and transpire rain water back to the atmosphere and to reduce the heat island effect of built surfaces.
- Infiltration planting beds and rain gardens around buildings and structures to filter and infiltrate roof and pavement runoff and to provide landscaped property amenities.
- Cisterns for rain water collection and reuse for landscape irrigation.
- Permeable paving in parking and loading areas to absorb and retain rain water.
- Bioretention cells and swales within and around paved areas to filter and infiltrate rain water runoff.
- Summer shading of hard surfaces and provide landscaped property amenities to minimize heat island effect.
- Naturalized swales within roadside parkways and medians to absorb and filter rain water and to provide natural character to the travel ways
- Regional naturalized ponds and detention areas to accommodate the 100-year design storm and to provide habitat and attractive water features throughout the development.
- Native and adaptive landscape systems to provide habitat and attractive landscape elements.

A regional approach to stormwater management for The Neighborhood could allow development zones to be concentrated in those areas that are most suitable for commercial and residential building while avoiding environmentally sensitive areas. By concentrating development in this manner, the cost of infrastructure installation and maintenance (primarily roads, utilities, and stormwater infrastructure) can be reduced while maintaining the development potential of the site.

The location of regional, naturalized detention areas should be based on existing topography, soils, size of watershed and efficiency of conveyance. Regional basins would be created through Development Agreements with the City and individual property owners or developers. Ideally, the regional basin improvements would be completed by the developer and a fee for the area of which the basin serves would be established. As the service area develops, fees would be collected, and depending on the agreement, either the City or the developer would be reimbursed for the land and improvement cost above and beyond the improvement to the initial development.



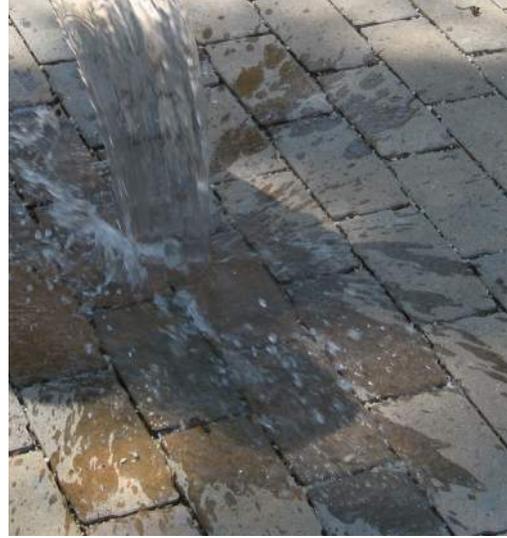
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### INFILTRATION PLANTING BEDS AND RAIN GARDENS

Water can be collected, filtered and reused throughout The Neighborhood in both formal and informal planting designs.



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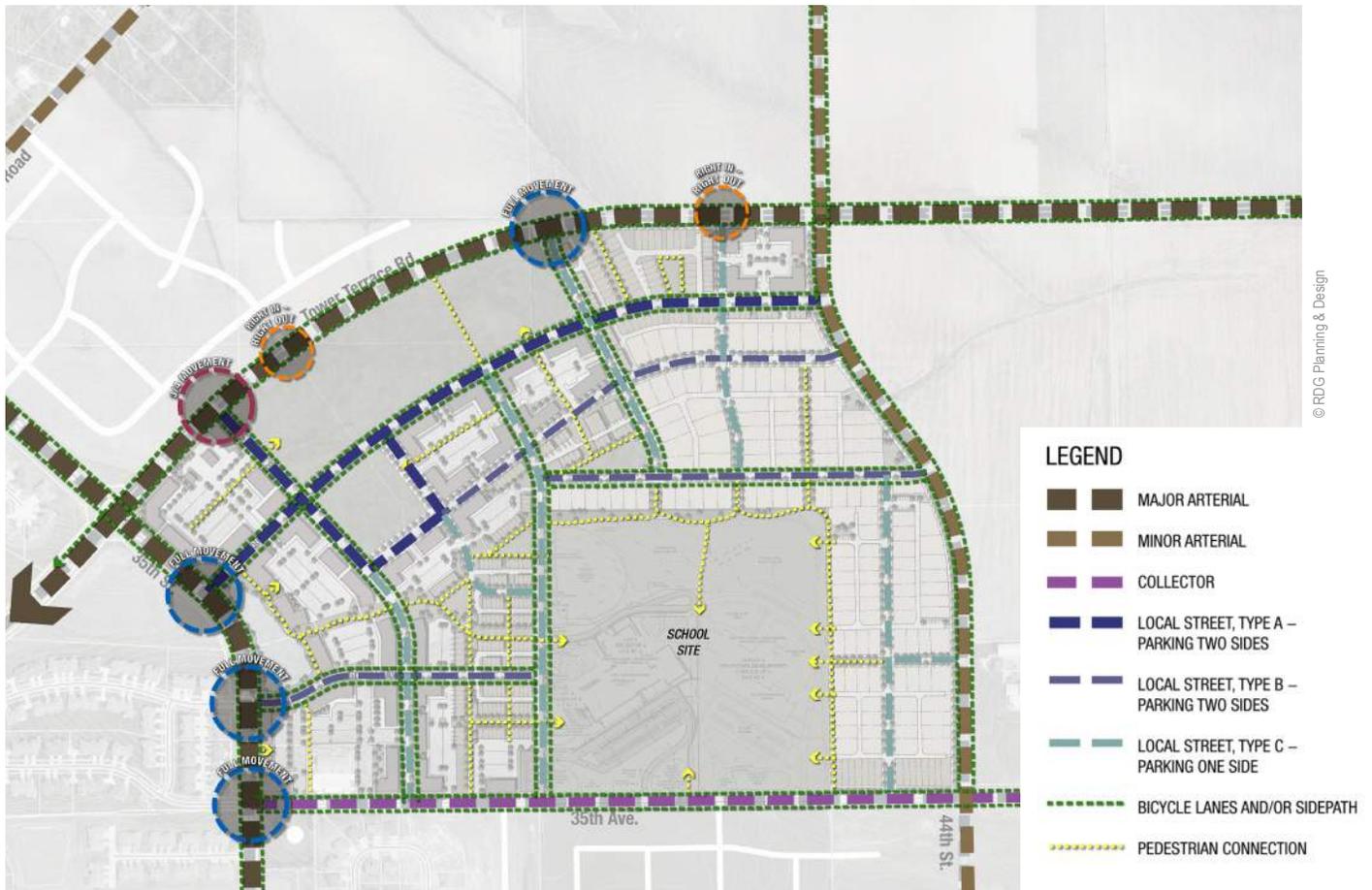


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### EXAMPLES OF STORMWATER BEST MANAGEMENT PRACTICES

The design of bioretention areas that utilize native and/or adaptive plantings with engineered soils and the implementation of pervious paving in parking and streetscape areas are two examples of BMP's that allow stormwater to be infiltrated where it falls, as opposed to sheet draining stormwater to drains and ultimately nearby water bodies.

## STREET DESIGN



**FIGURE 8: STREET DESIGN DIAGRAM**

The above diagram illustrates the proposed classification of the master planned streets and intersections within The Neighborhood at Indian Creek.

### INTENT

When developing Street Design Guidelines, it is important to consider all potential users of both the street and its adjacent right-of-way areas, as well as their mode of transport and frequency of use. Cars, trucks, tractor trailers, bicyclists and pedestrians all have distinct needs, and often times all must utilize the same transportation infrastructure – roadways, trails and walks immediately adjacent to one another - to safely travel to their individual destinations. One must consider projected traffic loads, the uniqueness of the project site, stormwater management, lighting, long-term maintenance, landscape

enhancements, level of streetscape development desired and potential phasing of the project when attempting to design complete streets. One goal of this design guidelines project is the cohesive development of a safe multi-modal network of streets, trails and walks within the development.

These guidelines will review a series of street design elements, through text and imagery, and make recommendations for the development of spatial and aesthetic guidelines within The Neighborhood at Indian Creek.

## **MINOR ARTERIALS**

### **44TH STREET**

Streets deemed to be minor arterials by these design guidelines are projected to be the conveyors of a lesser amount of traffic than the major arterial. Based on the current engineering design and the Major Streets Guide, 44th Street is envisioned to be a minor arterial. The illustrative section in Figure 9 represents the current engineering dimensions provided to the design team for this street. This design provides for two lanes along with bicycle lanes. A five foot sidewalk is recommended on the west side of the road, and an eight foot trail is recommended on the east side of the road.

## **COLLECTORS**

### **35TH AVENUE**

The streets deemed collectors by these design guidelines are projected to be the conveyor of a lesser amount of inter-development traffic than the major and minor arterial streets. The current engineering dimensions provided to the design team for this street are the same as the dimensions for 44th Street and is illustrated in Figure 9. This design provides for two lanes along with bicycle lanes. A five foot sidewalk is recommended on the north side of the road where not already built, and an eight foot trail is recommended on the south side of the road.

## **LOCAL STREETS**

### **LOCAL STREET, TYPE A**

The streets that are adjacent to areas envisioned as commercial spaces and adjacent to the community park or community square are designed to accommodate these adjacencies. Residents, shoppers and park users will be able to use the parallel parking on both sides of these streets. The widened landscape zones provide the framework for a comfortable and vibrant streetscape. See Figure 10 for an illustrative section of these two conditions. Some of the Local Streets, Type A, are recommended to also have bicycle lanes and/or sidepaths that serve to connect community destinations. See the Street Design Diagram in Figure 8 for these locations. All sidewalks shall have a minimum width of 5 feet.

### **LOCAL STREET, TYPE B**

This street type is designed to accommodate general vehicular traffic and primarily residential parking. Residents and some commercial users will be able to use parallel parking spaces located on both sides of the street. A generous landscape zone allows for a walkable environment. See Figure 11 for an illustrative section of this condition. Some of the Local Streets, Type B, are recommended to also have bicycle lanes and/or sidepaths that serve to connect community destinations. See the Street Design Diagram in Figure 8 for these locations. All sidewalks shall have a minimum width of 5 feet.

### **LOCAL STREETS, TYPE C**

Local streets, Type C, in this neighborhood are envisioned to become the connective tissue of the community. Narrow pavement widths, on-street parking on one side, and tree-lined walks are designed to provide a walkable atmosphere that enhances the overall neighborhood character and encourages residents to access neighborhood amenities without a vehicle. See Figure 12 for an illustrative section of this condition. Some of the Local Streets, Type C, are recommended to also have bicycle lanes and/or sidepaths that serve to connect community destinations. See the Street Design Diagram in Figure 8 for these locations. All sidewalks shall have a minimum width of 5 feet.

## **ROUNDBABOUTS**

During the Development Review, traffic studies will be conducted and roundabouts will be considered at all intersections. According to the Insurance Institute for Highway Safety, intersections with roundabouts can significantly improve traffic flow, reduce the number of vehicular crashes and improve pedestrian safety when compared to conventional intersections with stop signs or traffic signals.

### ALLEYS

To enhance the walkable character of The Neighborhood at Indian Creek, it is recommended by these design guidelines that residential blocks are served by alleys. Alleys provide access to utilities, waste receptacles and rear-loaded garages. Alleys also provide an additional access point for emergency vehicles. See Figure 17 on page 32 for an illustrative plan of a typical block with alleys.

### FIRE AND EMS ACCESS

All streets, alleys and parking lots shall permit the travel of the Fire Department's largest vehicle, including adequate accommodation of the vehicle's turning needs. If necessary, on-street parking may be prohibited near intersections on narrow streets to accommodate for the turning movement of large vehicles. Approval of unique design solutions to accommodate fire access may be granted by the City Council if the solutions proposed are recommended by the Marion Fire Department.

At the discretion of the City's Fire Chief, "No Parking Fire Lane" areas may be established as necessary to ensure efficient movement and access of the fire trucks, and includes no parking in alleys that the city maintains. The developers of The Neighborhood at Indian Creek should be responsible for the procurement and erection of approved fire lane signage.

All roads, alleys, and access drives shall maintain a minimum of twenty feet (20') of clear width and a minimum of fifteen feet (15') of vertical clearance at all times. However, alleys not considered fire access routes may be less than twenty feet (20') when approval by the City's Fire Chief, although all private lanes and alleys shall be in compliance with fire codes.

### ROADWAY AND STREET SIGNAGE

Public and private roads within The Neighborhood at Indian Creek shall be signed per City of Marion code.



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### STREETSCAPE CHARACTER EXAMPLES:

The above images illustrate potential street and streetscape design elements and concepts within The Neighborhood. Featured at the top is the view along Tower Terrace Road. Roundabouts and bike lanes are recommended, and roadway and street signage shall be incorporated where appropriate.



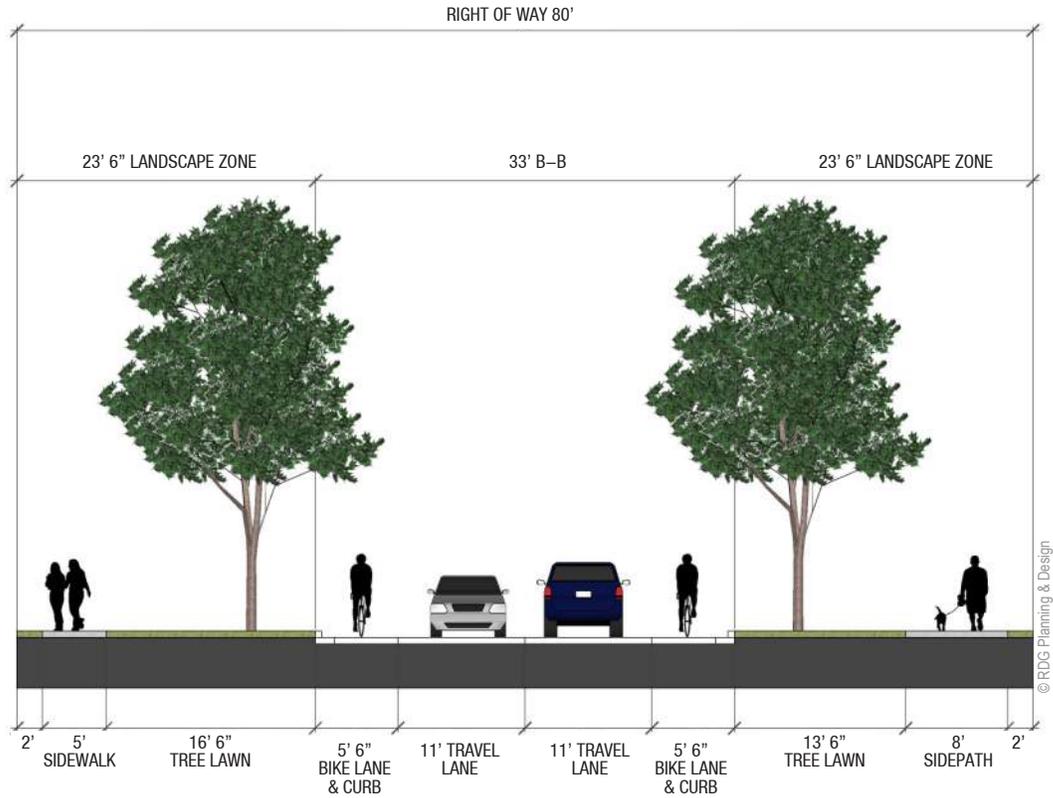


FIGURE 9: SECTION - 44TH ST. & 35TH AVE

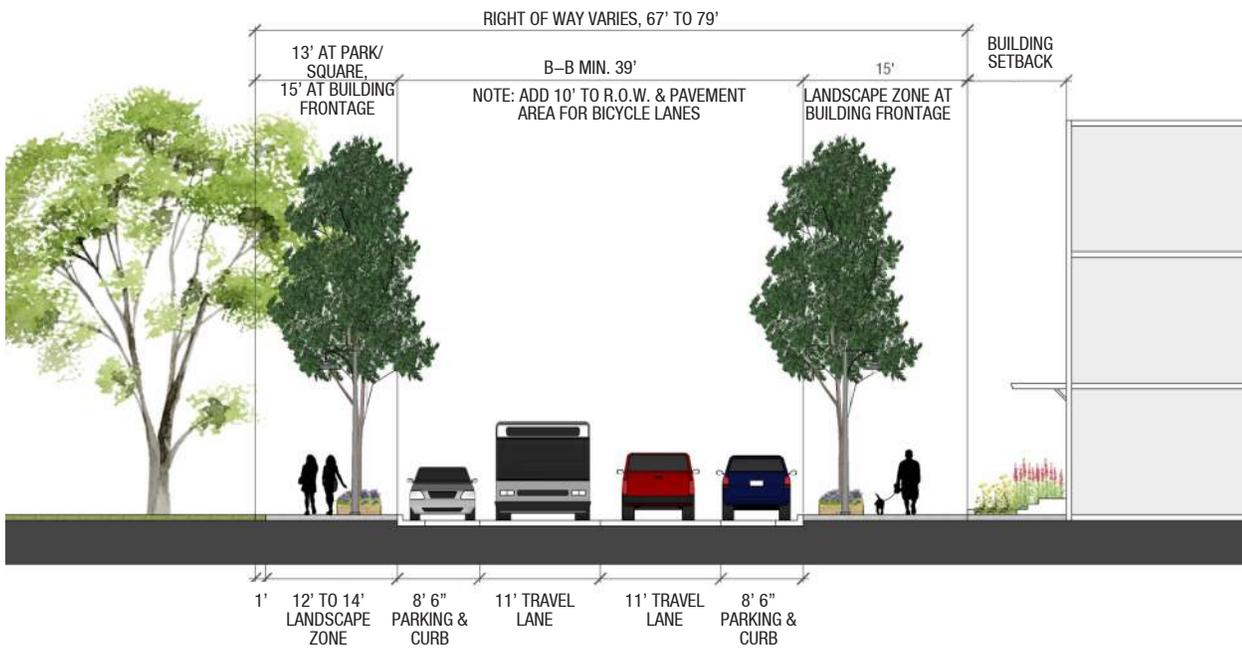


FIGURE 10: SECTION - LOCAL STREET, TYPE A

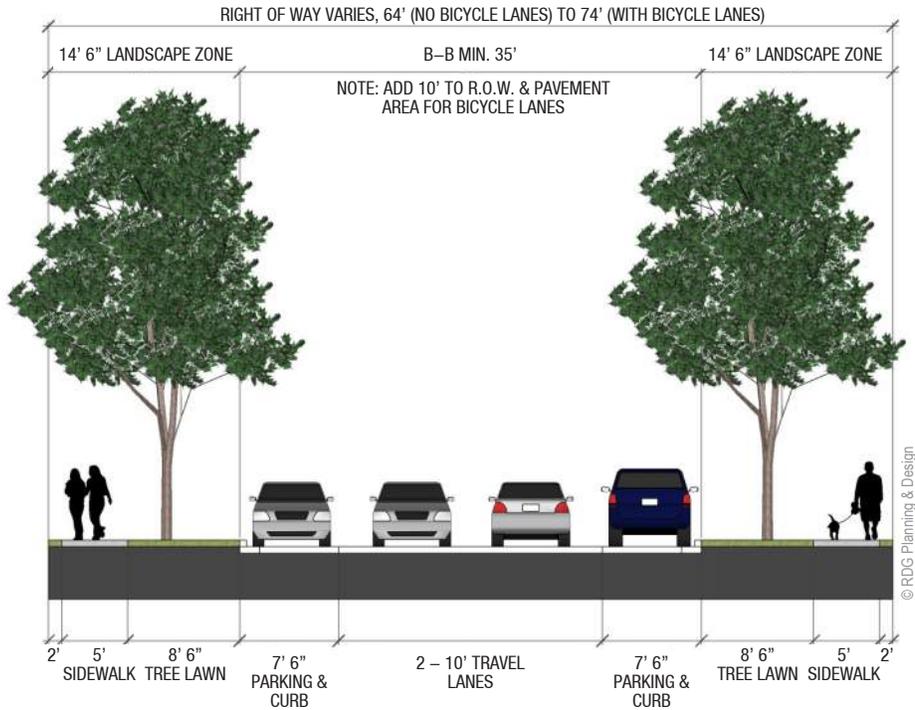


FIGURE 11: SECTION - TYPICAL LOCAL STREET, TYPE B

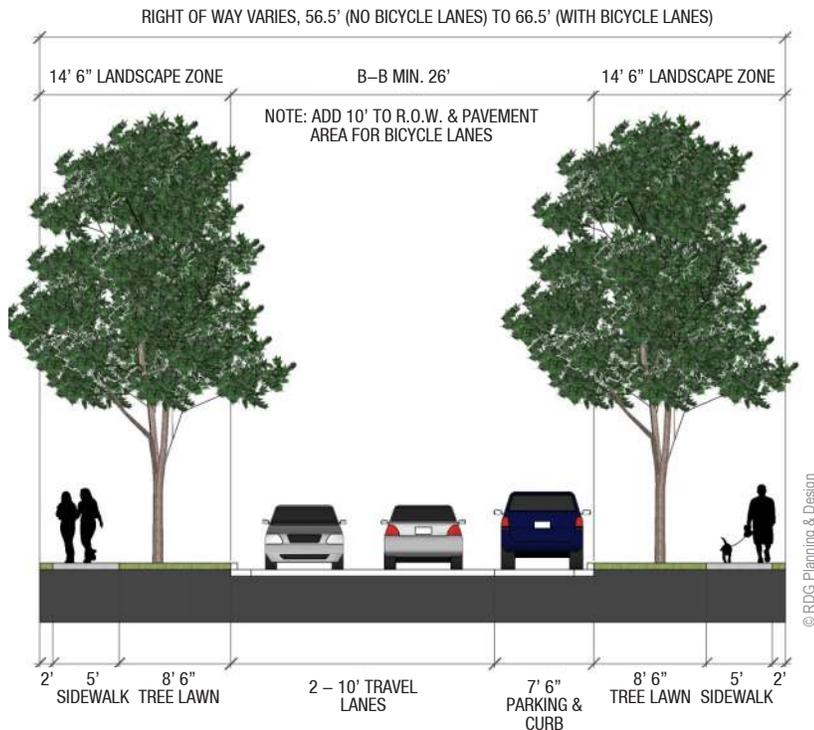


FIGURE 12: SECTION - TYPICAL LOCAL STREET, TYPE C

## PARKING

### INTENT

To provide safe and convenient pedestrian access to facilities served by the parking areas, to minimize the amount of parking necessary to serve a business or residences, and to ensure parking areas contribute to the character of The Neighborhood.

### BEST MANAGEMENT PRACTICES

The sharing of parking areas that are adjacent between businesses is required by this design guidelines document and is recommended for those in close proximity to one another. This shared use can minimize the amount of impervious surface on a lot or series of lots and therefore the amount of stormwater run-off that needs to be treated within the development.

Parking lots are required to provide stormwater enhancements to treat and convey the stormwater that falls on them.

The following practices are recommended where feasible and functional:

#### STORMWATER MANAGEMENT:

Pervious Paving

Bioretention Areas

Rain Gardens (Soils Permitting Infiltration)

The use of White or Gray Cement concrete paving is encouraged for parking lots within The Neighborhood because they have higher Solar Reflective Index (SRI) ratings. This means that they reflect solar radiation to a greater degree and minimize the local Heat Island Effect. The use of Asphalt paving for parking lots is not encouraged because it has a lower SRI rating meaning it absorbs solar radiation to a greater degree making the areas around parking lots warmer in the summer requiring more energy to cool the structures around them.

### LANDSCAPE

Parking lots shall provide both interior and perimeter landscape. Perimeter landscaping practices shall include a buffer at least 30" in height, or a 3' decorative fence. To achieve the buffer height, the perimeter landscaping can include:

#### PARKING PERIMETER LANDSCAPE ENHANCEMENTS:

Native or Adaptive Shade Trees

Adaptive Ornamental Trees

Native or Adaptive Shrubs

Native Grasses

Native or Adaptive Groundcovers

#### PARKING INTERIOR LANDSCAPE ENHANCEMENTS:

The following are required for plantings and landscape enhancements within parking lots in The Neighborhood at Indian Creek, as also detailed in the district standards:

- Parking islands shall not be less than ten feet (10') in width from back of curbs.
- Individual parking islands shall be a minimum of 160 square feet.
- Parking islands shall have a ground cover (i.e. sod, evergreen, native plantings) and include a minimum of one (1) shade tree.
- No parking space should be more than one hundred feet (100') from a landscaped area.
- Parking lots greater than one hundred thousand square feet (100,000 sf) in size should be divided into smaller parking fields that include a continuous parking island (paving and landscape) that divides the lot every fourth parking bay - designed to separate pedestrian traffic from vehicular traffic by including protected walkways that lead to building entrances. These protected islands should be a minimum of sixteen feet (16') in width from back of curb including a minimum six foot (6') wide walkway.



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**PARKING RATIOS BY LAND USE**

Parking ratios per land use are defined in the district standards in the appendix. Allowances are calculated based on the square footage of the use. If a use does not occupy the whole building, only the square footage devoted to the use shall be used in parking space calculations. In instances where it is necessary to exceed the required parking ratios, it is recommended that the excess parking be created using a pervious paving system.

**PARKING LOT SETBACKS**

It is recommended that all open air parking lots within the development be setback a minimum of twenty five feet (25') from Tower Terrace Road, twenty five feet (25') from 35th Street, and twenty five feet (25') from 44th Street. These setbacks can encompass stormwater drainage areas. Standards for parking lot setbacks for each Neighborhood district are detailed in the appendix.

**MIXED-USE & HIGH DENSITY PARKING LOT LOCATIONS**

Unless specifically allowed, all parking lots are constructed as interior parking courts within The Neighborhood Mixed Use and Multi-Unit Residential Districts. It is desirable that these parking lots be located in areas that are not directly viewable from the neighborhood square, 35th Street or Tower Terrace Road. The district standards address these siting requirements.

**RESIDENTIAL PARKING LOT LOCATIONS**

It is required that all surface parking areas, not including driveways leading to a garage, are located in a rear yard of a lot. It is desirable that parking lots be located in areas that are not directly viewable from 35th Street or Tower Terrace Road. The design of the neighborhood may have homes with frontage on an alley and a greenway. When determining parking locations, the alley is considered the rear of the lot.

**SCREENING**

All parking lots within The Neighborhood should be screened from public roadways through the use of landscape and plant material, as described previously under the landscaping recommendations and the standards in the appendix.

**GENERAL**

No parking shall be allowed on commercial driveways, walks or landscaped areas. Parking on residential townhome/single family home driveways is acceptable so long as the parking space is a minimum of twenty feet (20') long. The space shall not impede the flow of vehicular or pedestrian traffic and parking shall be located on hard surfaces at all times.



**FIGURE 13: SECTION – TYPICAL PARKING BAY WITH MIXED-USE BUILDING**



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### PEDESTRIAN ELEMENTS CONCEPTS

The images above are intended to illustrate park, square, and trail concepts that are recommended by this master plan.

### PEDESTRIAN ELEMENTS CHARACTER IMAGES

The images above are intended to illustrate a series of materials (concrete, stone, landscape, water) and manufactured elements (bicycle racks, tables, chairs and trash receptacles) that may be used throughout the development to aid in establishing a sense of place within The Neighborhood at Indian Creek.

## PEDESTRIAN ELEMENTS

### INTENT

Historically, the design of pedestrian areas was secondary to vehicular areas. Many times, if a pedestrian had a narrow sidewalk and a safe street crossing, the job of the designer was complete. However, over time this has changed. Now, more than ever, pedestrian circulation as well as the safety, comfort and aesthetic enhancement of pedestrian circulation routes and pedestrian spaces is expected. This expectation has come about as a result of an increased awareness that people's day-to-day environments do impact their quality of life. The more comfortable, safe and aesthetically pleasing a space is to a user, the greater the quality of the experience and the potential that the user could utilize that space or route again.

The intent of this section is to make a series of recommendations for the planning of these pedestrian routes and spaces. The recommendations will touch on a variety of spatial base-lines and aesthetic enhancements that could serve the future designers of these spaces.

### SIDEWALKS

All sidewalks and associated crossings of streets and access drives shall conform to the standards set forth in the Americans with Disabilities Act or City of Marion Code, whichever is more restrictive. It is required that all sidewalks within residential areas of The Neighborhood at Indian Creek are a minimum of five feet (5') in width. Sidewalk widths in the commercial areas and within park lands will vary, but should not be less than six feet (6') in width.

### PEDESTRIAN CROSSINGS

The crossing of roadway intersections and mid-block crossings, if they are desired, should be made evident to both pedestrians and vehicles. The use of contrasting colors and textured paving materials such as clay brick or precast concrete pavers or integrally colored and stamped concrete are recommended. All pedestrian crossings shall be in compliance with the Americans with Disabilities Act and the City of Marion code. All plant material, monuments, bollards and streetscape elements shall respect recommended vision triangles and City of Marion regulations as they regard the ability of that element to break-away in the event of a collision. Where appropriate, pedestrian crossings shall include traffic control features.

### TRAILS

All public trails within the Neighborhood at Indian Creek should conform to AASHTO (American Association of State & Highway Traffic Officials) standards or the City of Marion standards, whichever is more restrictive. All trails and associated crossings of streets and access drives shall conform to the standards set forth in the Americans with Disabilities Act or City of Marion Code, whichever is more restrictive.

### PAVING

The paving of all public sidewalks and trails within The Neighborhood at Indian Creek are to be constructed using Portland Cement Concrete. Integrally colored, stained concrete and precast concrete pavers are also acceptable. Decomposed granite, limestone fines or other ADA accessible pervious paving materials may be utilized for private walkways.

### SITE FURNISHINGS

The incorporation of site furnishings such as seating and trash receptacles aid in defining a space through comfort and aesthetics. It is recommended that when developing a space, whether a retail streetscape or park land area, the inclusion of these elements be considered for their functionality, but also because their appearance aids in unifying the whole of the development. Site furnishings should be uniform (color, style and material) throughout The Neighborhood. While furnishings do not necessarily have to be exactly the same throughout, they should appear to belong to the same family.

### PLAZA SPACES

The inclusion of formal spaces where people are free to gather in large and small groups will likely increase the usability of The Neighborhood and add to the life of the development outside the walls of business. The design of these spaces should allow for universal accessibility whenever possible and should consider the use of authentic materials where they are likely to be touched by users. Color, texture, form, and material are all important aspects of a successful space.

## LANDSCAPE

### INTENT

Landscape Design and Open Space guidelines for The Neighborhood at Indian Creek should work toward the goal of a cohesive neighborhood aesthetic. The intent of this section is to establish baseline recommendations for the design of landscape and open space within The Neighborhood at Indian Creek.

Landscaping along streets and pedestrian connections at The Neighborhood at Indian Creek is recommended as follows:

#### ARTERIAL STREETS:

##### **35th Street and Tower Terrace**

Landscaping along arterial streets, as held by these design guidelines, should adhere to the following planting recommendations where they are not utilized for stormwater management.

#### **Median Planting**

- (3) Shrubs should be recommended for every (100) square feet of median area available for planting.
- A (20) square foot mass native grass or grass and forbs planting may be substituted for (2) Shrubs.
- Where vehicular site distances preclude the use of shade trees or taller shrubs, lower plant material should be substituted.

#### **Landscape Zone Planting**

(1) Shade Trees should be planted approximately (40') on center on each side of the street.

#### LOCAL STREET, TYPE A:

Landscaping along streets designated as “Local Street, Type A” as held by Figure 10 should adhere to the following planting recommendations where they are not utilized for stormwater management.

- (1) Shade Trees should be planted approximately (40') on center on each side of the street.
- Shade trees should maintain an upright habit for adequate pedestrian and vehicular movement.
- Public shade trees shall be selected from a list of predetermined shade trees allowed in the city right-of-way because of their shade and root characteristics. Additionally, tree species which have adequate transparency to allow for views to building signage and to allow some filtered sunlight along the streetscape should be selected.
- A minimum of 10% of the ground plane of the landscape zone shall be permeable areas comprised of planting beds or above-grade landscape planters.

#### LOCAL STREET, TYPES B & C:

Landscaping along collector streets and local streets, as held by these design guidelines, should adhere to the following planting recommendations where they are not utilized for stormwater management.

- (1) Shade Trees should be planted approximately (40') on center on each side of the street.
- Tree variety shall not be the same for more than one block along a street corridor.



### NATIVE AND ADAPTIVE PLANTINGS IN FORMALIZED LANDSCAPES

The use of native Iowa and adaptive plant materials is recommended in all aspects of landscape development. The desire within the development is to create an aesthetically pleasing courtyard, plaza, building entry area or landscape buffer through the use of native Iowa and adaptive perennials and overstory and understory trees. There is a wide variety of color and form to be found in the multitude of native Iowa and adaptive species.

**RETAIL PEDESTRIAN CONNECTIONS:**

Mid-block pedestrian connections of a minimum (30') building-to-building width should be provided between buildings in the Neighborhood Mixed Use district. These connections should provide a memorable and comfortable pedestrian entrance into and exit from the heart of the community. Refer to the illustration below for a typical retail pedestrian connection. Each connection should adhere to the following recommendations:

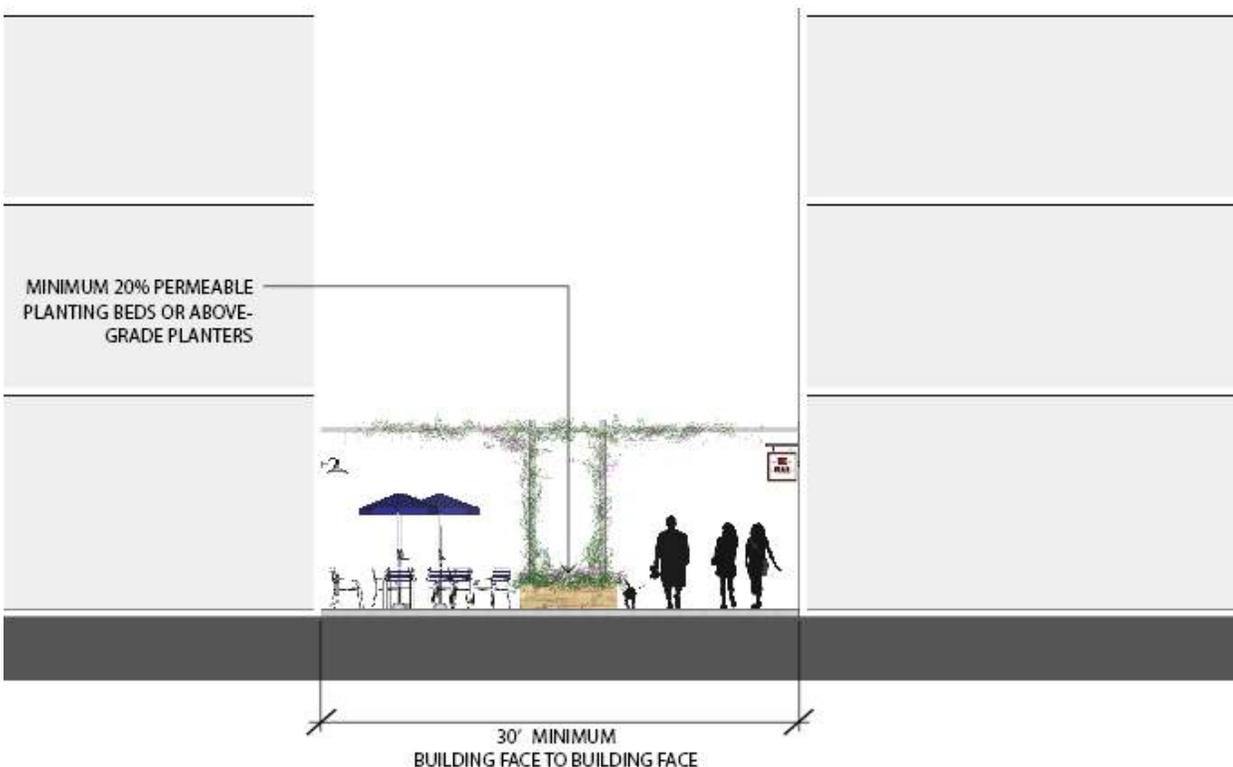
- A minimum of 20% of the ground plane of the landscape zone should be permeable areas comprised of planting beds or above grade landscape planters.
- Connections should be designed to provide opportunities to gather, rest and dine, where appropriate.
- Connections could integrate public art, water features and overhead canopies.
- Building façades adjacent to connections should extend street-fronting façade architecture into the pedestrian connection with building elements such as windows, awnings, doors and signage.

**SCHOOL PEDESTRIAN CONNECTIONS:**

Mid-block pedestrian connections through the school site should be provided to encourage and allow for a safe route to school. Private development shall coordinate with these connections by providing public sidewalks in alignment with the connections.

**OPEN SPACE**

Open space within The Neighborhood at Indian Creek should be considered in its greater context and designed holistically. Whatever the space's purpose, it should not be considered to be "left over" or designed as though it stands alone. Open space within The Neighborhood is intended as a unifying element. These spaces can be either passive, active or both depending upon where they are located within the development and their intended use. In the case of the community park, public open space will facilitate the gathering of many people, convey pedestrian and bicycle traffic away from vehicular traffic, establish areas for quiet reflection, and in some areas of the park, provide a gathering space for groups.



**TYPICAL RETAIL/MIXED-USE PEDESTRIAN CONNECTION**



## DEVELOPMENT SIGNAGE

### INTENT

Signage plays dual roles in the making of a place. Signage must effectively communicate to a user. It may communicate to the user their location in a larger context, the location of a business they may be seeking or simply that they are now entering a distinct space.

Beyond wayfinding or contextual location, a sign also aids in the definition of a place and, in the instance of a gateway, the definition of a space. The sign should respect its site and intent of the development in which it is located. See the Figure below for identification signage locations.

### GATEWAY ARCHITECTURAL ELEMENTS

These portals of entry and exit are areas of great importance. They work to convey the identity of the development to those entering and exiting The Neighborhood. Architectural elements within gateway areas could be constructed using materials outlined for use in the appendix. Scale of elements should be appropriate to their location, adjacent architecture, vehicles and pedestrians.



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**FIGURE 14: NEIGHBORHOOD IDENTIFICATION/GATEWAY SIGNAGE LOCATION DIAGRAM:** The above diagram depicts potential locations along primary vehicular and pedestrian corridors where identification signage could be located. This signage should be of an appropriate scale that allows passers-byes to identify the area they are arriving at or passing through. All signage should be designed to be of similar character (color, texture, form, materials, etc.) to the surrounding development.

## LIGHTING

### Commercial and Arterial Examples



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### INTENT

Lighting plays two roles in the making of a place. First, it must function. The proper illumination of a space adds to the level of comfort a user has when in that space. Second, when the luminaire is not providing light, it aids in creating a sense of unity through repetition to a place through its form.

The Neighborhood should have separate, distinguishable lighting from other areas in Marion. The selection of poles and luminaires that complement the site, architectural styles and energy conservation goals of The Neighborhood at Indian Creek could be important to creating a sense of place. Commercial areas on Tower Terrace Road and 35th Street need to be well lit for pedestrian activity. Residential areas need more subtle lighting, but still show the unique character of The Neighborhood. Existing federal, state and local energy codes should be consulted and adhered to in all instances.

### Sign Examples



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### Residential Examples



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© Lithonia Lighting

Lighting fixtures and poles shall complement the Tower Terrace Road lighting. Typically, there could be at least three different lighting types within the development - a roadway/alley light, a parking lot light and a pedestrian light. The style and illumination of each type may vary between commercial and residential areas. Wherever a light may be found in the development, it is important that it relates to all others. This relation can happen through a common color, style, scale and/or material. The common mounting of a light on an architecturally detailed base may also aid in bringing a sense of unity to the lighting throughout the development.

Light selection should be partially based on power usage and requirements. It is a goal of this development to minimize the use of energy in all aspects of design. It is recommended that when selecting lighting for the development and individual sites the designer make every effort to incorporate solar cell and LED technology and other energy saving measures into their selection criteria. When designing parking lot lighting, it is recommended that consideration is given to lowering parking lot lighting levels outside of business hours.

One goal of this development is to reduce, if not eliminate, light pollution and glare within the development. Lighting design should eliminate hot spots, glare along roadways and light spillage onto adjacent properties. Fixtures that do not direct light to its intended area of focus and/or do not have cut-offs are discouraged.

### THE NEIGHBORHOOD AT INDIAN CREEK SIGNAGE AND LIGHTING

The images above illustrate what signage and lighting within the development may look like in commercial and residential areas. While safety and security are paramount, the aesthetic value of light must be reinforced.

## ARCHITECTURAL CHARACTER AND MATERIALS

### INTENT

It is the goal of these standards to establish a method for the development of a cohesive architectural aesthetic in The Neighborhood. These standards and guidelines work to balance the goal of creating a neighborhood atmosphere through complementary form, material, color, and pattern with the goal of avoiding monotony of architectural style. The following palette of construction materials, thoughtful mix of architectural styles and orientations of structures, coupled with the standards found throughout this document, will work to develop a sense of community and place.

This intent is achieved through materials standards, bulk regulations, and the use of particular architectural elements consistent with the development vision. Ultimately, the overall design and detailing of structures should include elements which create opportunities for a cohesive community aesthetic and reinforce a comfortable human scale throughout The Neighborhood. These character elements are listed in the district standards and Appendix 3 as:

- Building orientation on the site and placement of front entry
- Building height, width, and setbacks
- Building scale and massing
- Building materials and texture
- Building variety

Note that the material and architectural design standards are not meant to limit creativity. Materials used for exterior finishes are changing all the time, and exceptions can be granted at times for functionally and aesthetically similar materials.

### SITE DESIGN

The successful design of a site will directly impact the success of its residential and commercial development. A viable mixed use concept should involve the intelligent assembly of commercial development within effective, pedestrian oriented urban design. A quality environment will be defined by how safely and efficiently it functions while remaining aesthetically pleasing amid its surroundings. The standards for each proposed district listed in the appendix work to create this environment.

Components of the standards include thoughtful building placement, pedestrian orientation, adequate parking, and well planned drive-thru and building service facilities. Commercial design should define the user experience with pedestrian drop-off zones, flexible and functional streetscape, walkways and landscape, as well as pedestrian elements such as lighting, art, fences, walls and site furnishings integrated into the streetscape, plazas and green space.

Similarly, proper residential site design must address viewsheds, solar angles, prevailing winds and stormwater with the close placement of houses to each other, streets and alleyways. As importantly, the collection of houses over time must maintain walkable connections to green corridors, trails and commercial areas. Houses and buildings are positioned closer to the streets, establishing a pedestrian scale that encourages foot traffic. Building height, setbacks and spacing define the character of streets, greenways, parks and open areas that comprise The Neighborhood.



### EXAMPLES OF RESIDENTIAL ARCHITECTURE AND PATTERN

The above images and those on the following page are indicative of residential architectural forms, scales, materials and styles that might be found in compliance with the standards in The Neighborhood at Indian Creek.

## MATERIALS

The consistent use of exterior materials throughout The Neighborhood is an important part of creating a sense of unity, cohesion, and ultimately “place.” This development is expected to react to its immediate context and respect the uniqueness of its site. As such, certain building materials are required for exterior building finishes for all structures and listed in the appendix. A team of architects evaluated the list to capture high quality, lasting materials that are widely used in building design. The standards describe that building elevations facing public streets and spaces shall be comprised of more primary materials - those deemed high/ lasting quality and desirable for The Neighborhood. Secondary materials are appropriate in smaller portions on public facing elevations, but allowed more liberally on elevations not visible from public streets and spaces.



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## RESIDENTIAL CHARACTER IMAGERY

The above images and those on the previous page are indicative of residential architectural forms, scales, materials and styles that might be found within The Neighborhood at Indian Creek.

Although application will vary, the use of these materials should be recognizable in the exterior character of each structure. The primary materials are intended to be used on all building elevations, including those primarily viewed from within a property and/or from an adjacent property, as well as those visible from the public right of way. These building materials are recognizable natural elements of quality and durability when utilized in the proper configuration and have been utilized in the Marion area for some time.

Allowable colors for any acceptable material include earth tones, neutrals and primary colors at the discretion of the City of Marion. Vibrant primary colors could be allowed to accent architecturally significant building detail, but must contribute to the buildings overall attractiveness and design. Brick and stone should be colored only by means of pigment impregnation throughout the entire material and not be painted only on the exterior surface. Additionally, noticeable variations in color, pattern and texture resulting from casting, manufacturing, fabrication, etc. of exterior building materials should not be allowed.

Additional materials and colors that are considered key to a retail tenant’s identity may be used, provided that they conform to the recommended accents and are utilized in such a manner consistent with the intention of these standards and guidelines.

## RESIDENTIAL DESIGN STYLE

A key to the successful residential development of The Neighborhood will be the variety of housing types and styles available to potential buyers. The Plan includes lot sizes for row houses, townhomes, multi-plexes, and a variety of single family home sizes. The Urban Residential district standards described in the appendix list the variety of residential uses allowed. Several styles recommended and encouraged may include Victorian, Prairie, Arts and Crafts, Tudor, Four Square and Colonial. To ensure diversity in building style, development in The Neighborhood shall not have blocks with expanses of buildings of the same material, color, and style in a row. Appendix 3 describes the methods required for building variety in The Neighborhood. Ultimately, the city should work cooperatively with interested residential builders and real estate professionals to understand the ability of local professionals to deliver a quality product at a relevant price point within the intent of this design manual.



**FIGURE 15: PROTOTYPICAL SINGLE FAMILY HOUSING**

The above image illustrates the potential aesthetic of proposed single family housing.



**FIGURE 16: MIXED USE COMMERCIAL AT NEIGHBORHOOD SQUARE**

The above image illustrates the potential aesthetic of the Mixed Use Commercial surrounding the neighborhood square within the development.

### ARCHITECTURAL DESIGN STANDARDS: NON RESIDENTIAL AND MULTI-FAMILY RESIDENTIAL USES

Buildings shall not include blank walls without architectural design and treatments. Instead, buildings should maintain a human scale along all elevations which can be viewed from public rights-of-way and public spaces such as parks and plazas. The following treatments, used together, are acceptable examples to break up blank walls and to make larger buildings feel like a series of smaller facades:

- Varying roof heights and shapes;
- Varying decorative parapet heights and shapes;
- Use of multiple wall materials, textures, and colors to create a horizontal aesthetic;
- Use of projecting building elements such as column or pillar vertical offsets, overhangs, and shading devices;
- Separate building elements such as free-standing screen walls;
- Designed building offsets over two feet in depth; and
- Planters, benches, or other furniture near the building.

Additional architectural design standards that shall apply to all buildings:

- Buildings shall be oriented so that the front elevation (the elevation with the building's main entry) faces the road/street or greenway serving the building but not the alleyway.
- Main entrances to buildings shall be a significant architectural feature of the building's design and be easily identified as such from the public road or greenway that serves the building.
- Main entrances of all buildings shall open onto an outdoor, pedestrian-scaled space that provides a comfortable transition between the building and the site parking (when present). Pedestrian pathways shall connect into this space. Buildings which have multiple tenants may open onto a shared outdoor space with shared walks and amenities that provide pedestrian friendly gathering areas.

Lastly, quality building design and materials are important, and so is environmentally friendly design. Buildings are recommended to be oriented with their long elevations to face north or south to maximize potential for day-lighting opportunities and to employ functionally designed and energy responsible elements such as overhangs, sunscreens/awnings and other shading devices (which may include perforated metal panels or fabric structures); clerestory glazing or roof monitors (energy efficient alternative to standard skylights) to accommodate an interior day-lighting strategy; and light colored (heat reflecting) membrane roofs, and green (vegetative) roofs.

### ACCESSORY STRUCTURE DESIGN STANDARDS

Accessory structures such as trash enclosures and mechanical equipment screens shall be made from the same primary materials listed in the appendix. Gates shall be made of an approved form of architectural metal on a metal frame. Colors for these structures should match the surrounding architecture, and should not draw attention through the use of accent materials or colors and/or detract from the area aesthetics.



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## IMPLEMENTATION

These design standards and guidelines can provide a framework for the creation of a vibrant and unique new neighborhood for the City of Marion; however, the plan will not lead to success if the design standards and guidelines are not properly applied or only partially implemented. The update of The Neighborhood at Indian Creek Master Plan does support the goals of the City as it does align with the Marion Comprehensive Plan, Complete Street Policy, Parks Master Plan, Trails Master Plan, and Step into Nature Plan, all adopted by Marion City Council.

Staff will review each development against the Master Plan to ensure that the intent of the Master Plan is met through architectural character, circulation (vehicular and pedestrian), stormwater management, walkability, and parks and open space accommodations.

Any developer(s) desiring to develop any project within The Neighborhood area will need to submit a scaled site plan and color façade elevations consistent with the Master Plan, including proposed exterior materials to be used. After City Staff reviews the submittal, the developer will have an opportunity to make any changes in response to Staff’s comments. Staff will make a recommendation to the Planning & Zoning Commission regarding whether the site plan and structure(s) are being developed “in the spirit” of The Neighborhood at Indian Creek Master Plan’s vision. The Planning and Zoning Commission will then make a recommendation to City Council for final consideration.



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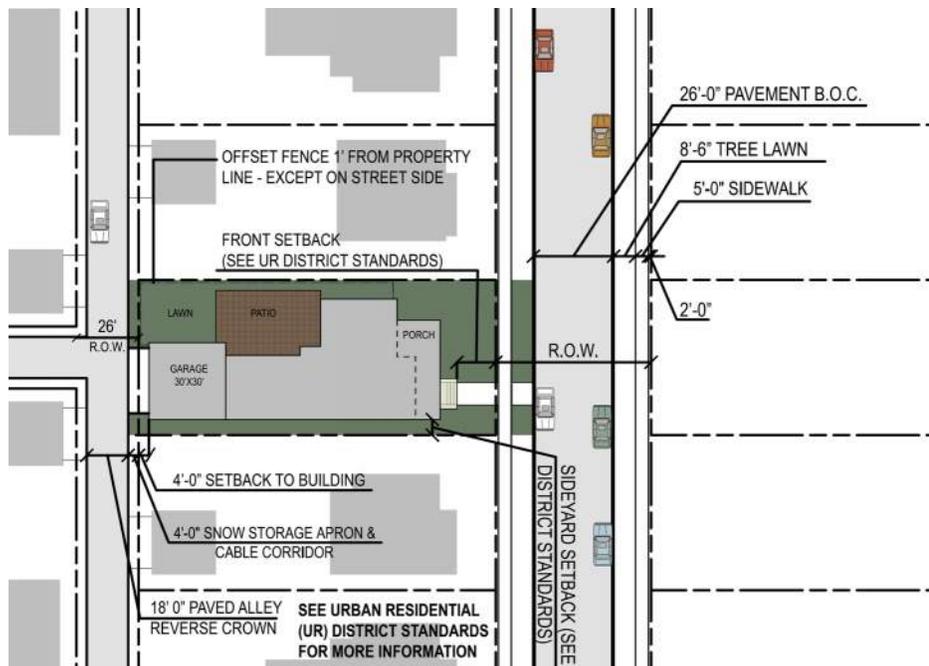
## APPENDIX 1: SUPPLEMENTAL DIAGRAMS



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**FIGURE 16: PROPERTY OWNERSHIP LOT LINES**

The above diagram illustrates the current and proposed lot lines of property ownership for the lands underlying the master plan of The Neighborhood at Indian Creek study area.



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**FIGURE 17: TYPICAL RESIDENTIAL LOT LAYOUT**

The above diagram illustrates a typical single family residential lot layout.



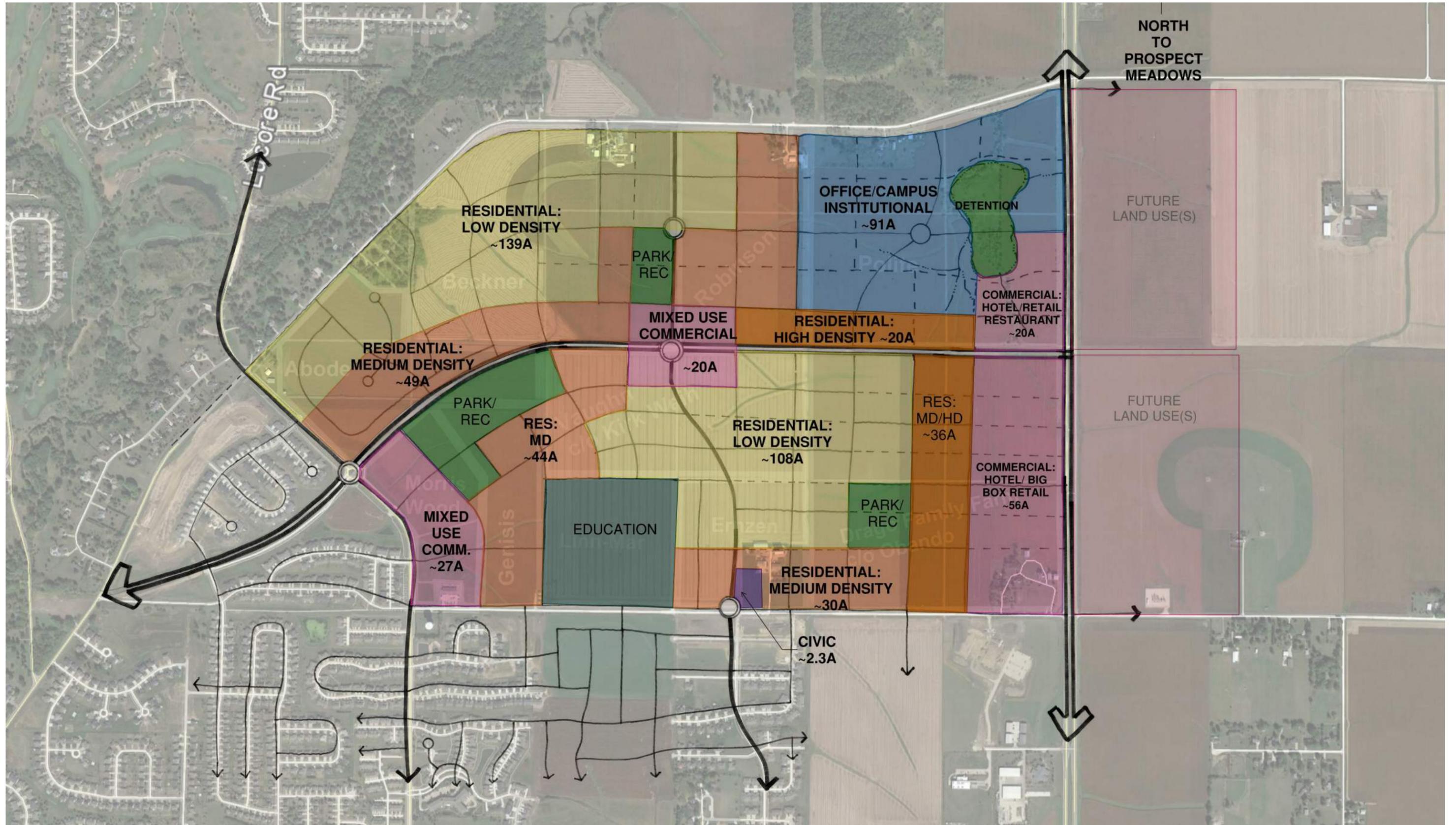
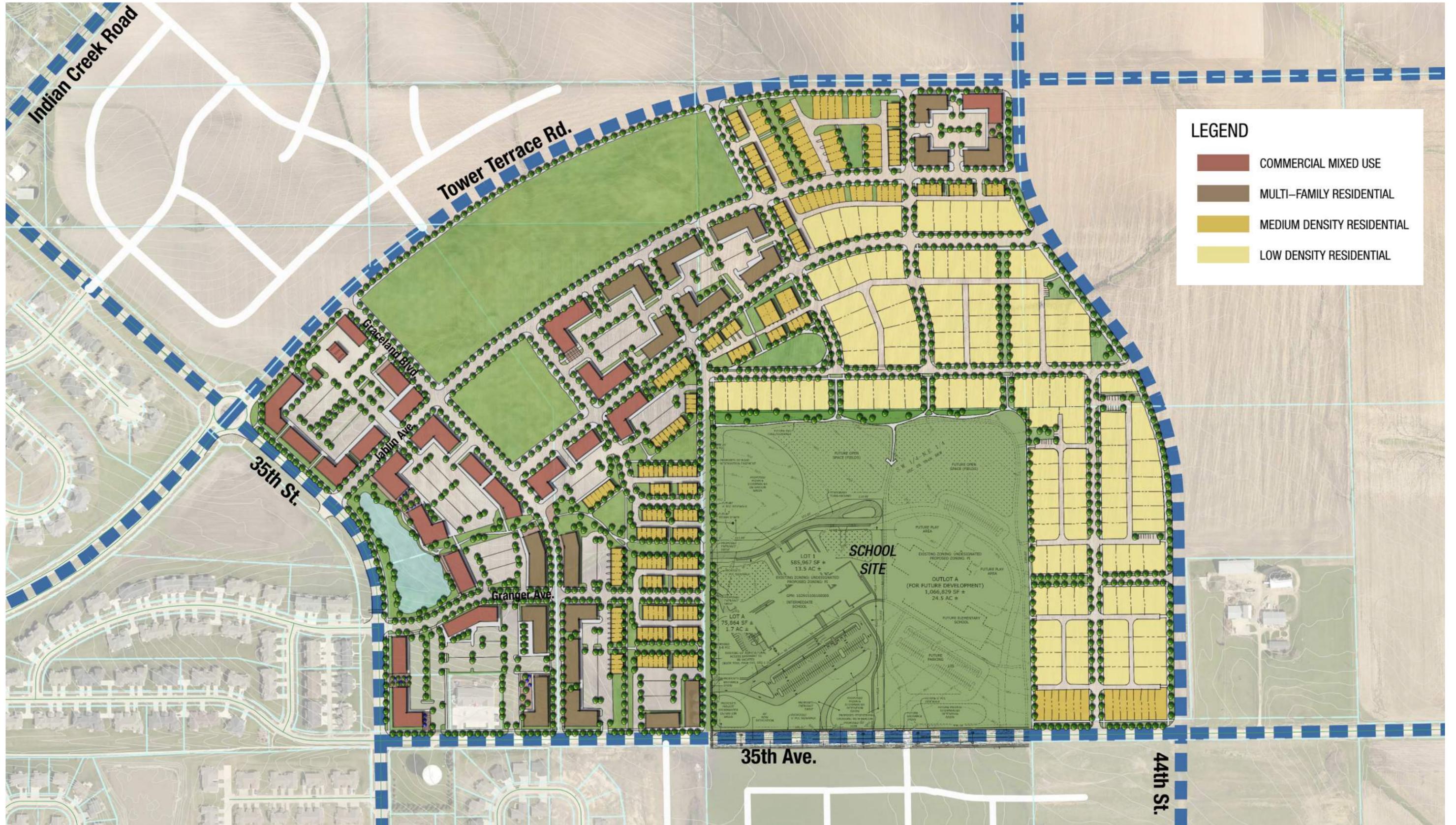


FIGURE 18: MASTER PLAN IN GREATER CONTEXT

The above diagram illustrates the larger land use plan for the area bounded by Indian Creek Road to the north, 35th Street to the west, 35th Avenue to the south, and Highway 13 to the east.

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**LEGEND**

- COMMERCIAL MIXED USE
- MULTI-FAMILY RESIDENTIAL
- MEDIUM DENSITY RESIDENTIAL
- LOW DENSITY RESIDENTIAL

FIGURE 19: ENLARGED MASTER PLAN  
The above diagram illustrates the Master Plan.

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## APPENDIX 2: DISTRICT STANDARDS SHEETS

### INTENT/PURPOSE

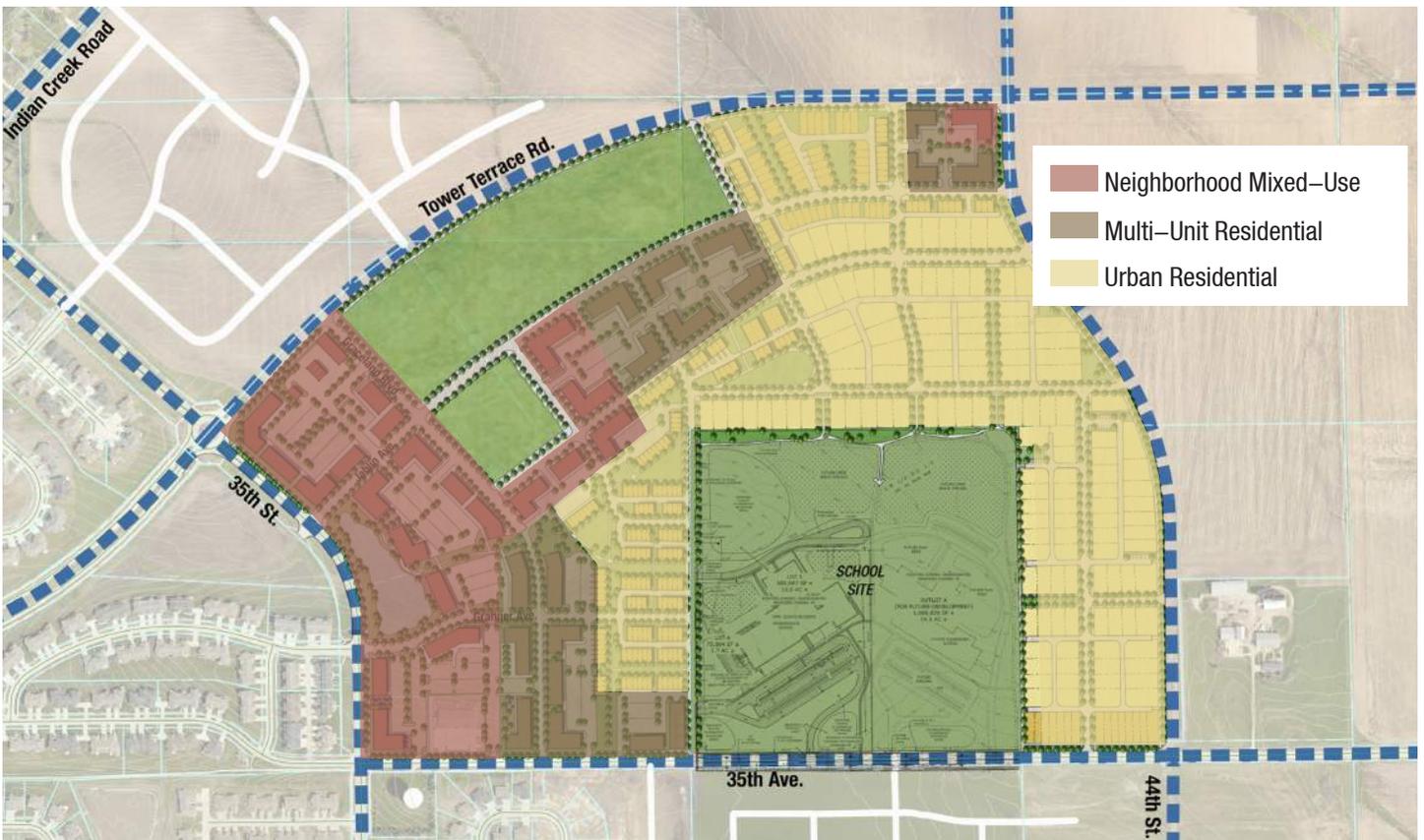
The following pages describe the specific standards that shall be applied to development in The Neighborhood. Development applications shall be subject to the design review procedures identified under the City of Marion Ordinance.

The standards develop controls that result in structures within the development that are in scale with and provide transitions to the surrounding properties and uses that:

- Do not significantly block views and sunlight
- Are of a human scale and create a sense of space
- Provide the perception of public accessibility to the development
- Provide for development at a scale that is economically feasible
- Allow for development throughout The Neighborhood at Indian Creek.

As stated and shown at the beginning of the design manual, the following districts and standards are established:

- Neighborhood Mixed Use (MX)
- Multi-Unit Residential (MUR)
- Urban Residential (UR)

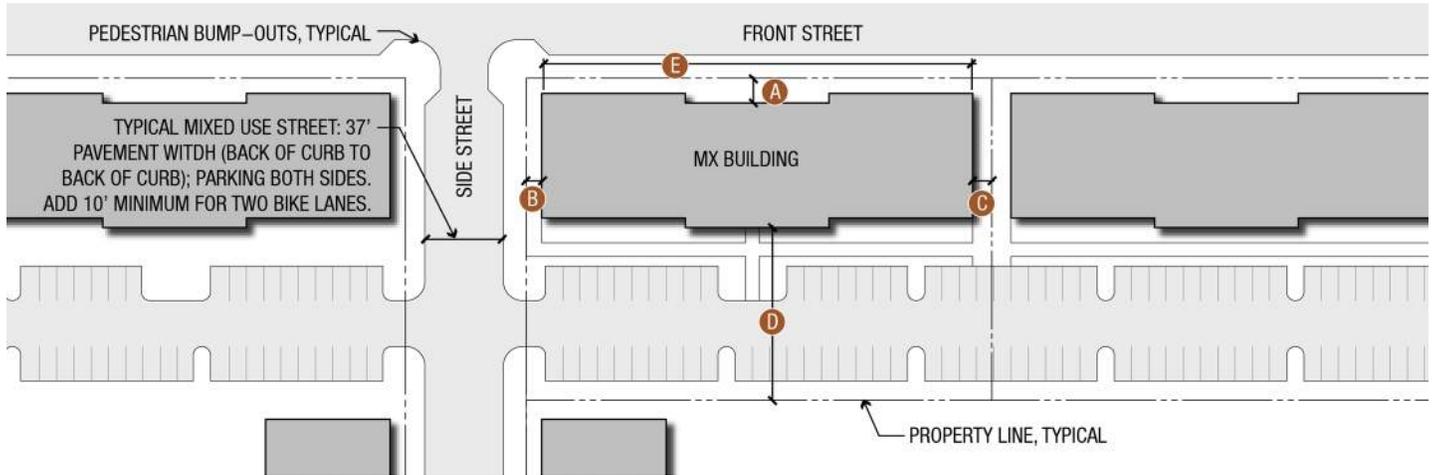


**FIGURE 20: RECOMMENDED DISTRICT APPLICATION**

The plan above is a diagrammatic representation of the recommended district land uses.

## A2.1 - NEIGHBORHOOD MIXED USE (MX) STANDARDS

The MX district encompasses the areas within the Neighborhood at Indian Creek Master Plan located generally east of 35th Street, north of 35th Avenue, south of Tower Terrace Road and west of Graceland Boulevard. The MX district is an area where commercial services, retail uses, and residential uses are encouraged to develop in a horizontally and vertically mixed-use environment. Higher densities and intensities will be provided for and are allowed in this area. The intent is to realize a mixed use development with Midwestern architectural character that functions as a center for the surrounding neighborhood(s).



### BUILDING PLACEMENT

#### Principal Building Setback (Distance from Property Line)

A	Front	15' Maximum
B	Side Street, Corner Lot	15' Maximum
C	Side	0' Minimum
D	Rear	
	Adjacent to Residence	15' Minimum
	Adjacent to any other use	10' Minimum

#### Accessory Building Setback

Front/Side Street, Corner Lot setback	≥ Principal bldg setback
Side/Rear	≥ Principal bldg setback or 5' Minimum

#### E Building Form<sup>1,2</sup>

Building frontage at property line <sup>3</sup>	Front: 80% Minimum Side Street: 50% Minimum
Lot Width	N/A
Lot Depth	N/A
Site Impervious Coverage	80% Maximum

#### USES PER FLOOR - VERTICALLY MIXED<sup>4</sup>

1st Floor	Commercial
2nd Floor	Commercial/Residential
3rd Floor	Commercial/Residential

### HEIGHT<sup>5</sup>

Building Minimum	20'
Building Maximum	3 stories

### BUILDING FACADES

#### Materials

Facing ROW or Open Space	75% Primary Materials
Not facing ROW	50% Primary Materials
Materials are identified in the Design Guidelines Manual	

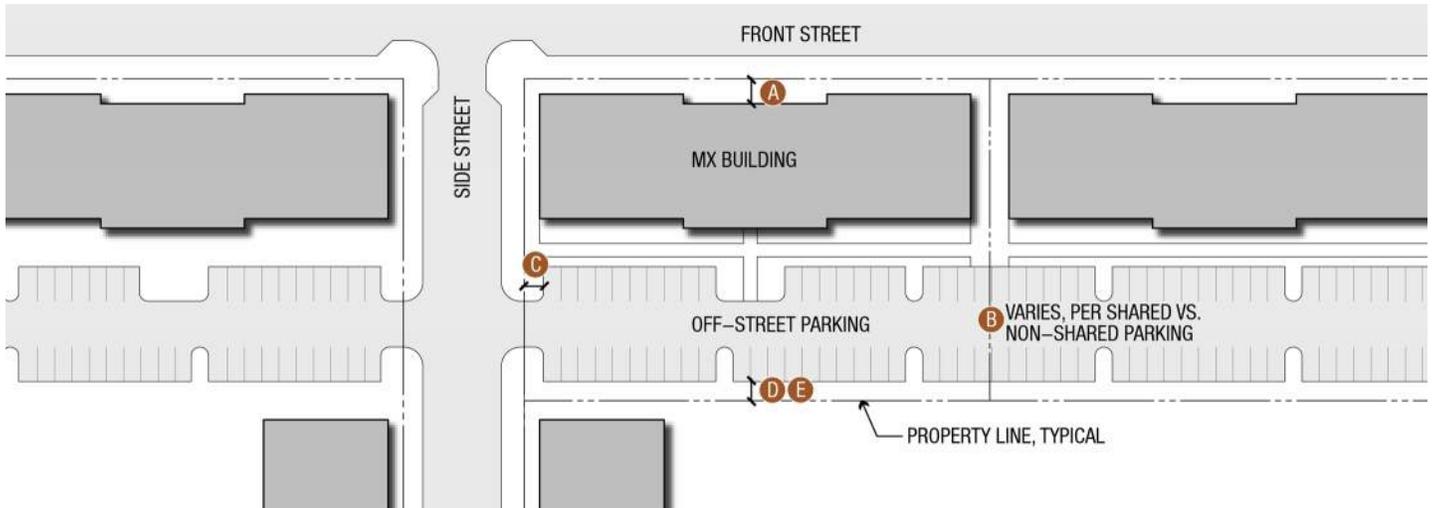
### Design and Architecture<sup>6</sup>

Any building over 75' wide must be broken down to read as a series of buildings no wider than 75' each. Allowed techniques are identified in the Design Guidelines Manual.

#### Notes

1. All buildings must have a primary ground-floor entrance that faces a primary or side street.
2. Rear-facing buildings, loading docks, overhead doors, and other service entries are prohibited on street-facing facades.
3. The percentage of the width of the lot that shall be fronted with buildings.
4. Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.
5. All heights measure to eaves or top of parapet.
6. Mansard roof forms are not allowed.

## A2.1 - NEIGHBORHOOD MIXED USE (MX) STANDARDS, CONT.



### PARKING

#### Location (Distance from Property Line)<sup>1, 2, 3</sup>

Front Setback	<b>A</b> Parking not allowed
Side Setback <sup>4</sup>	<b>B</b> 0' Minimum when shared w/adjacent lot 5' Minimum when not shared
Side Street Setback <sup>5</sup>	<b>C</b> 10' Minimum
Rear Setback <sup>4</sup>	0' Min. when shared w/adjacent rear lot <b>D</b> 10' Minimum when not shared
Adjacent to Residential	<b>E</b> 10' Minimum

#### Required Spaces<sup>6</sup>

Mixed-Use Commercial/Office	3.5 space/1,000 GSF Maximum
Mixed-Use Residential <sup>7</sup>	1.5 space/Dwelling Unit Minimum
Medical Office	1.0 space/150 GSF Maximum
Bicycle Parking	1.0 space/10 required vehicle spaces Located near building entrances

#### Lot Design<sup>8</sup>

Parking Design	See Section 176.29
Lighting	Shaded and oriented downward
Perimeter Landscaping	Required when adjacent to residential, public streets. Natural ground cover planted w/shrubs, trees, or berm that forms a buffer at least 30" in height, or a 3' decorative fence.

Interior Landscaping Minimum 160 SF for each landscaped island. All spaces within 100' of landscaped area. Refer to the Design Guidelines Manual for location requirements

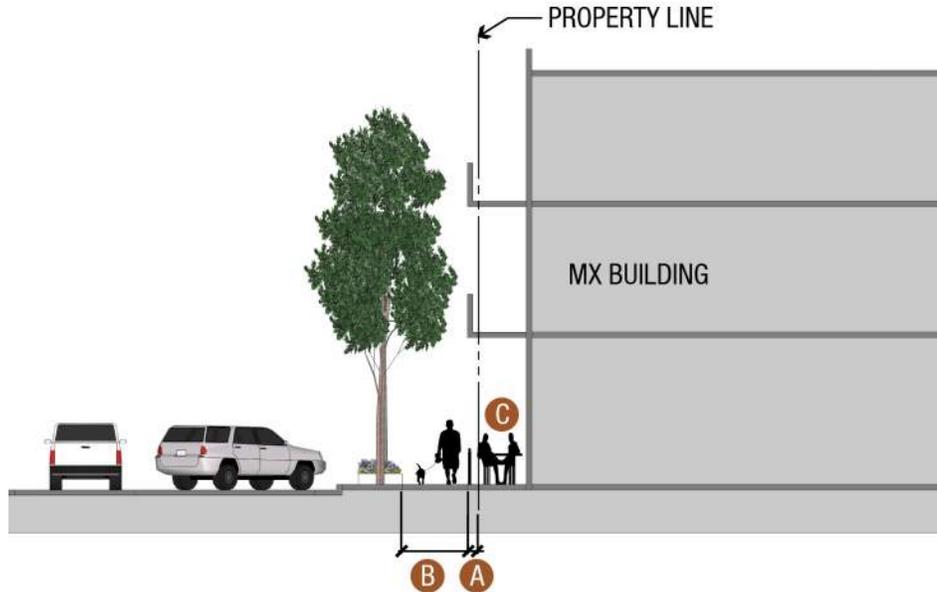
### SCREENING

Roof top equipment	Fully screened from view
Ground level equipment <sup>9</sup>	Enclosed or screened by fence/plantings
Trash enclosures <sup>9</sup>	6' tall opaque masonry wall Architectural metal gate

### Notes

1. Parking not allowed between building and public streets. Parking only allowed to the back or side of buildings.
2. All setbacks must be landscaped with a natural ground cover, excluding any pedestrian walkways.
3. Parking not allowed on the ground floor of buildings or garages.
4. Shared drives are encouraged between adjacent lots to minimize curb cuts along the street.
5. On corner lots, primary parking access shall not be located on primary street.
6. Parking may be provided off-street within 300' or as shared parking.
7. Three or more bedroom units require two parking spaces per unit.
8. Paved walkways shall be provided from all parking areas to building entrances or other pedestrian systems.
9. Must be located on the rear of the building or lot.

## A2.1 - NEIGHBORHOOD MIXED USE (MX) STANDARDS, CONT.



### ALLOWED SIGNAGE<sup>1</sup>

Sign Types	Height	Area	Number
Wall <sup>2</sup>	< building	15% of total facade area	1 per building or tenant
Window <sup>3</sup>	N/A	30% of each window area	1 per tenant
Informational/ Directional <sup>4</sup>	4'	6 sf	1 per entr/exit
Temporary	Refer to section 176.31, Marion Zoning Ordinance		
Multi-Tenant	25' max	1 sf per linear ft. street frontage <sup>5</sup>	1 per street frontage <sup>5</sup>

All other signage standards shall be in accordance with section 176.31 under the City of Marion Zoning Ordinance.

### ENCROACHMENTS<sup>6, 7</sup>

Yard	Encroachment Location	Allowance
<b>A</b> Front	ROW Encroachment <sup>8, 9, 10</sup>	2' Maximum
<b>A</b> Side Street	ROW Encroachment <sup>8, 9, 10</sup>	2' Maximum
Side	Not allowed	N/A
Rear	Setback Encroachment	5' Maximum
Height		Unlimited

### Notes

1. Illuminated signs shall be either 'halo lit' or externally illuminated through 'gooseneck' type lighting fixtures directed at the sign.

2. Includes awning, canopy, and projecting signs. Wall signs shall project not more than twelve (12) inches from a wall and projecting signs shall project no more than six (6) feet from a wall with a clearance of eight (8) feet from the sidewalk. For multiple-tenant buildings, combined sign area of all tenants is 15% of total facade area.

3. Limited to ground floor non-residential uses.

4. Can be freestanding, or attached to buildings or light poles. Shall not interfere with walkways.

5. Maximum 100 sf Can only be located along primary streets identified in the Design Guidelines Manual.

6. Limited to canopies, awnings, balconies, eaves, cornices, chimneys, elevator bulkheads, outdoor dining, and solar energy systems.

**B** 7. Ground floor encroachments should not impact or reduce unobstructed pedestrian walkways below 6'.

8. Encroachments into the right-of-way require additional approval and permitting in accordance with the City Engineering Department.

**C** 9. Outdoor dining can encroach further given 6' of unobstructed pedestrian walkways are provided and all requirements imposed by the State of Iowa are met, if serving alcohol.

10. Canopies, awnings, and balconies may encroach up to 5' into the ROW on front and side street yards if located at least 8' above grade.



## A2.1 - NEIGHBORHOOD MIXED USE (MX) STANDARDS, CONT.

### GROUND FLOOR FRONTAGE TYPES ALLOWED

General Shopfront	Comprised of 50% non-tinted glass
Canopies	Clearance 8' min. back from curb line Height 8' min. clear, 2 stories max.
Awnings	Clearance 8' min. back from curb line Depth 10' max.
Forecourts	Depth 20' min., not to exceed width Width 20' min., 50% of lot width max.

## A2.1 - NEIGHBORHOOD MIXED USE (MX) STANDARDS, CONT.

TABLE A2.1: Mixed Use (MX) Permitted and Conditional Land Uses

### LAND USE TYPE <sup>1</sup>

#### Residential

Artist Live/Work Space	P
Bed & Breakfast	C
Day Care Home	C
Dwelling, Efficiency/Micro Units <sup>2</sup>	P
Dwelling, Multi-unit 5+ units <sup>2</sup>	A
Dwelling, One-Unit <sup>2</sup>	P
Dwelling, Two-Unit <sup>2</sup>	P
Dwelling, Three/Four-Unit <sup>2</sup>	P
Dwelling, Townhome	P
Dwelling, Upper Level	P
Group Home	P
Home Occupation	P

#### Retail and Service<sup>3</sup>

Adult Daycare	P
Bank	P
Car Wash	C
Child Daycare	P
Drive-Thru	C
Financial Institution	P
Gas Station	C
Grocery - Neighborhood	C
Grocery	C
Hotel/Motel	C
Liquor/Tobacco Store (1st Floor)	C
Medical/Dental Clinic	P
Microbrewery	P
Office, Business and Professional	P
Outdoor Sales, Temporary <sup>4</sup>	A
Personal Services, limited	P
Restaurant	P
Retail Sales - Small	P
Retail Sales - Medium	P
Retail Sales - Specialty	P
Tattoo Parlor	P
Tavern/Bar	P

### LAND USE TYPE <sup>1</sup>

#### Cultural, Religious, Recreation & Entertainment

Cultural Facility	P
Indoor Recreation and Amusement	P
Live Entertainment	A
Lodge or Private Club	P
Outdoor Entertainment, Temporary	A
Park, Neighborhood	P
Place of Worship	P
Public Plaza	P
Theatre	P

#### Governmental and Institutional

Library	P
Office, Government	P
Post Office	P
Public Service Facility	C
School, Personal Services	C

#### Other

Parking Garage/Structure	C
Parking Lot, Private	C
Parking Lot, Public	P
Urban Agriculture <sup>5</sup>	P
Utility, Community/Regional	P
Utility, Local	C

#### Key

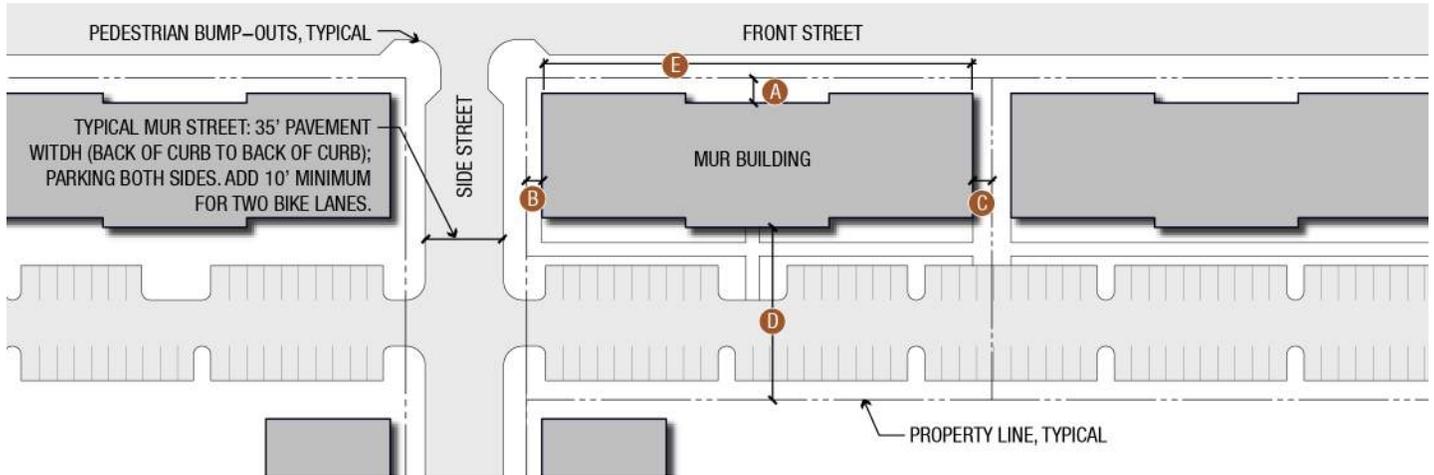
- P = Permitted Use
- C = Conditional Use
- A = Permitted as Accessory Use

#### Notes:

1. A definition of each listed use type is in Section 176.06 of the Marion Zoning Regulations.
2. Limited to upper stories.
3. Drive-thru uses require a conditional use permit and can only be a single lane allowing the stacking of four vehicles.
4. Outdoor dining allowed during business hours.
5. Limited to composting; farmers market; roof level urban farms; rooftop greenhouse; urban farm, roof level, small; vertical agriculture; and subject to the requirements of section 176.49.

## A2.2 - MULTI-UNIT RESIDENTIAL (MUR) STANDARDS

The MUR district encompasses the areas within the Neighborhood at Indian Creek Master Plan that generally abut the MX district to act as a buffer between the UR district. The MUR District is an area where high density residential is encouraged such as apartments, condominiums, and terrace communities. Commercial uses may be allowed on ground floors at intersections. The intent is to realize a traditional neighborhood development complemented by community commercial and civic uses.



### BUILDING PLACEMENT

#### Principal Building Setback (Distance from Property Line)

A	Front	15' Maximum
B	Side Street, Corner Lot	15' Maximum
C	Side	0' Minimum
D	Rear	
	Adjacent to UR District	15' Minimum
	Adjacent to any other use	10' Minimum

#### Accessory Building

Front/Side Street	≥ Principal bldg setback
Side/Rear	≥ Principal bldg setback or 5' Minimum

#### Building Form<sup>1,2,3</sup>

E	Building frontage at property line <sup>4</sup>	Front: 80% Minimum Side Street: 50% Minimum
	Lot Width	N/A
	Lot Depth	N/A
	Site Impervious Coverage	80% Maximum

#### HEIGHT<sup>5</sup>

Building Minimum	20'
Building Maximum	3 stories

### BUILDING FACADES

#### Materials

Facing ROW or Open Space	75% Primary Materials
Not facing ROW	50% Primary Materials
Materials are identified in the Design Guidelines Manual	

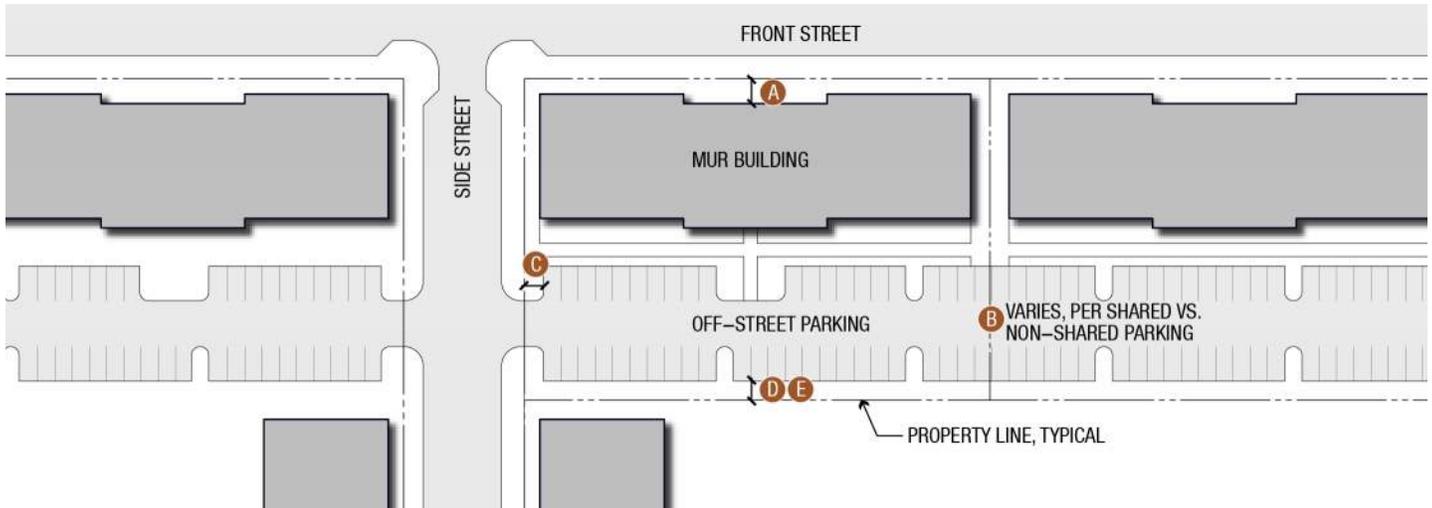
#### Design and Architecture<sup>6</sup>

Allowed techniques are identified in the Design Guidelines Manual.

#### Notes

- All buildings must have a primary ground-floor entrance that faces a primary or side street.
- Rear-facing buildings, loading docks, overhead doors, and other service entries are prohibited on street-facing facades.
- Buildings greater than 16 units must provide adequate common space for residents in the form of community rooms, roof terraces or courtyards.
- The percentage of the width of the lot that shall be fronted with buildings.
- All heights measure to eaves or top of parapet.
- Mansard roof forms are not allowed.

## A2.2 - MULTI-UNIT RESIDENTIAL (MUR) STANDARDS, CONT.



### PARKING

#### Location (Distance from Property Line)<sup>1, 2, 3</sup>

Front Setback	<b>A</b> Parking not allowed
Side Setback <sup>4</sup>	<b>B</b> 0' Minimum when shared w/adjacent lot 5' Minimum when not shared
Side Street Setback <sup>5</sup>	<b>C</b> 10' Minimum
Rear Setback <sup>4</sup>	0' Min. when shared w/adjacent rear lot <b>D</b> 10' Minimum when not shared
Adjacent to Residential	<b>E</b> 10' Minimum

#### Required Spaces<sup>6, 7</sup>

All Residential	1.0 space/Dwelling Unit Minimum
Mixed-Use Commercial/Office <sup>8</sup>	3.5 space/1,000 GSF Maximum
Bicycle Parking	1.0 space/10 required vehicle spaces Located near building entrances

#### Lot Design<sup>9</sup>

Parking Design	See Section 176.29
Lighting	Shaded and oriented downward
Perimeter Landscaping	Required when adjacent to residential, public streets. Natural ground cover planted w/shrubs, trees, or berm that forms a buffer at least 30" in height, or a 6' decorative fence.

Interior Landscaping	Minimum 160 SF for each landscaped island. All spaces within 100' of landscaped area. Refer to the Design Guidelines Manual for location requirements
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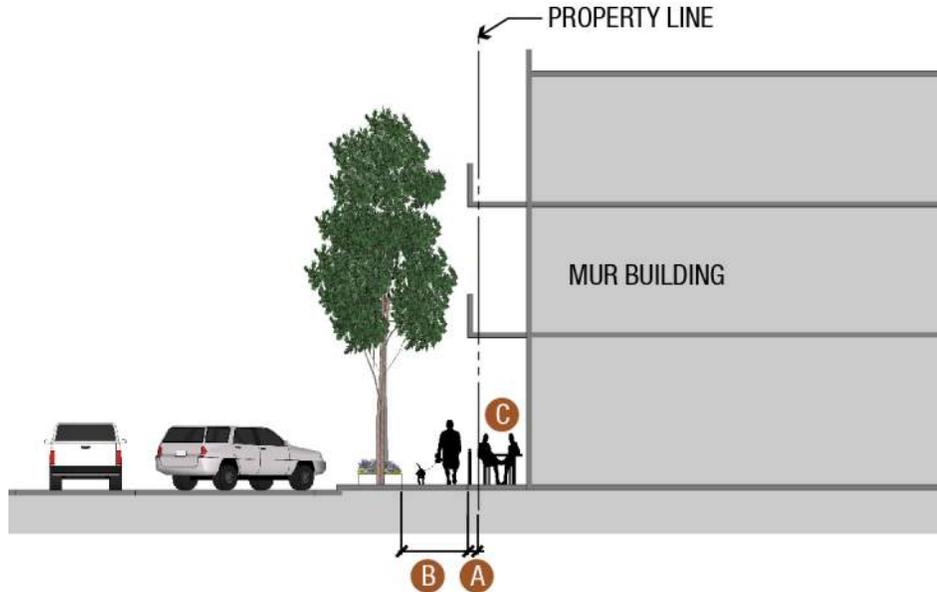
### SCREENING

Roof top equipment	Fully screened from view
Ground level equipment <sup>10</sup>	Enclosed or screened by fence/plantings
Trash enclosures <sup>10</sup>	6' tall opaque masonry wall with architectural metal gate

#### Notes

1. Parking not allowed between building and public streets. Parking only allowed to the back or side of buildings.
2. All setbacks must be landscaped with a natural ground cover, excluding any pedestrian walkways.
3. Parking not allowed on the ground floor of buildings or garages.
4. Shared drives are encouraged between adjacent lots to minimize curb cuts along the street.
5. On corner lots, primary parking access shall not be located on primary street.
6. Parking may be provided off-street within 300' or as shared parking.
7. Three or more bedroom units require a minimum of two parking spaces per unit.
8. See Table A2.2 for Land Use Type limitations.
9. Paved walkways shall be provided from all parking areas to building entrances or other pedestrian systems.
10. Must be located on the rear of the building or lot.

## A2.2 - MULTI-UNIT RESIDENTIAL (MUR) STANDARDS, CONT.



### ALLOWED SIGNAGE<sup>1</sup>

Sign Types	Height	Area	Number
Wall <sup>2</sup>	< building	25 s.f.	1 per tenant
Window <sup>3</sup>	N/A	30% of window area	1 per tenant
Awning	N/A	30% of total exterior surface	1 per tenant
Informational/ Directional <sup>4</sup>	4'	6 s.f.	1 per entr/exit
Temporary	Refer to section 176.31, Marion Zoning Ordinance		

All other signage standards shall be in accordance with section 176.31 under the City of Marion Zoning Ordinance.

### ENCROACHMENTS<sup>5, 7</sup>

Yard	Encroachment Location	Allowance
A Front	ROW Encroachment <sup>6, 8, 9</sup>	2' Maximum
A Side Street	ROW Encroachment <sup>6, 8, 9</sup>	2' Maximum
Side	Not allowed	N/A
Rear	Setback Encroachment	5' Maximum
Height		Unlimited

### GROUND FLOOR FRONTAGE TYPES ALLOWED

Awnings/Shopfront	Clearance 8' min. back from curb line Depth 10' max.
Forecourts	Depth 20' min., not to exceed width Width 20' min., 50% of lot width max.
Stoops	Depth 4' min

### Notes

- Internally illuminated signs are not allowed.
- Wall signs shall project not more than twelve (12) inches from a wall.
- Limited to ground floor non-residential uses.
- Can be freestanding, or attached to buildings or light poles and not used for commercial messages. Shall not interfere with walkways.
- Limited to awnings, balconies, eaves, cornices, chimneys, elevator bulkheads, and solar energy systems.
- Encroachments into the right-of-way require additional approval and permitting in accordance with the City Engineering Department.
- B 7. Ground floor encroachments should not impact or reduce unobstructed pedestrian walkways below 6'.
- C 8. Outdoor dining can encroach further given 6' of unobstructed pedestrian walkways are provided and all requirements imposed by the State of Iowa are met, if serving alcohol.
9. Awnings, and balconies may encroach up to 5' into the ROW on front and side street yards if located at least 8' above grade.

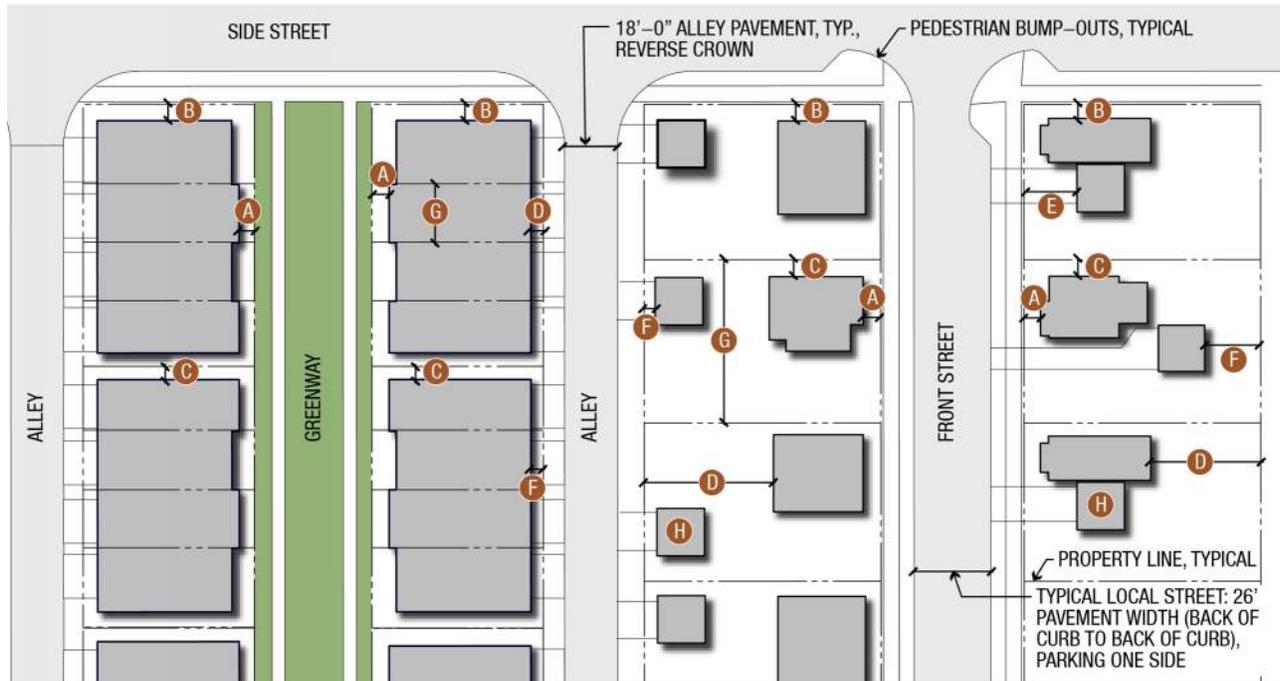
## A2.2 - MULTI-UNIT RESIDENTIAL (MUR) STANDARDS, CONT.

TABLE A2.2: Multi-Unit Residential (MUR) A Permitted and Conditional Land Uses

<b>LAND USE TYPE <sup>1</sup></b>		<b>LAND USE TYPE <sup>1</sup></b>	
<b>Residential</b>		<b>Other</b>	
Artist Live/Work Space	C	Parking Garage/Structure	C
Bed & Breakfast	C	Parking Lot, Private	C
Day Care Home	C	Parking Lot, Public	P
Dwelling, Efficiency/Micro Units	P	Urban Agriculture <sup>4</sup>	P
Dwelling, Multi-unit (5+) units	P	Utility, Community/Regional	P
Dwelling, One-Unit	P	Utility, Local	C
Dwelling, Two-Unit	P		
Dwelling, Three/Four-Unit	P		
Dwelling, Townhome	P		
Dwelling, Upper Level	P		
Group Home	P		
Home Occupation	C		
<b>Retail and Service<sup>2</sup></b>		<b>Key</b>	
Bank	P	P = Permitted Use	
Daycare Center	P	C = Conditional Use	
Financial Institution	P	A = Permitted as Accessory Use	
Grocery - Neighborhood	C		
Outdoor Sales, Temporary <sup>3</sup>	A		
Personal Services, limited	P		
Restaurant	P		
Retail Sales - Small	P		
Retail Sales - Specialty	P		
Tavern/Bar	P		
<b>Cultural, Religious, Recreation &amp; Entertainment<sup>2</sup></b>		<b>Notes:</b>	
Cultural Facility	P	1. A definition of each listed use type is in Section 176.06 of the Marion Zoning Regulations.	
Indoor Recreation and Amusement	P	2. Limited to the first 100' of frontage extending from corner intersections composed of MX or MUR districts.	
Live Entertainment	A	3. Outdoor dining allowed during business hours.	
Outdoor Entertainment, Temporary	A	4. Limited to composting; farmers market; roof level urban farms; rooftop greenhouse; urban farm, roof level, small; vertical agriculture; and subject to the requirements of section 176.49.	
Park, Neighborhood	P		
Place of Worship	P		
Public Plaza	P		
Theatre			
<b>Governmental and Institutional</b>			
Library	P		
Office, Government <sup>2</sup>	P		
Post Office <sup>2</sup>	P		
Public Service Facility <sup>2</sup>	P		

## A2.3 - URBAN RESIDENTIAL (UR) STANDARDS

The UR district encompasses the areas within the Neighborhood at Indian Creek Master Plan designated as low-medium density residential in the land use plan. The UR district is an area where a mix of low to medium density residential uses are encouraged such as single-family dwellings, duplexes, and townhomes. The intent is to realize a traditional neighborhood development complemented by community commercial and civic uses. The style of the residential uses should provide a cohesive look and feel to the district.



### BUILDING PLACEMENT

#### Principal Building Setback (Distance from Property Line or alley easement)<sup>1</sup>

	Single-Family	2-4-plex	Townhome
A Front <sup>2</sup>	6' min - 18' max	→	
B Side Street	6' min - 18' max	→	
C Side	5' min <sup>3</sup>	→ 0' between townhome building units 5' min at end of townhome buildings	
D Rear	5' min	→	

#### Accessory Building and Attached Garage Setback<sup>4</sup>

E Front/Side Street	20' in front of garage doors, min. All other 5' more than primary bldg facade		
F Side/Rear	5' min <sup>5</sup>		

#### Building Form<sup>6</sup>

	Single-Family <sup>7</sup>	2-4-plex	Townhome <sup>8</sup>
G Lot Width	35' min - 85' max	35' min - 140' max	18' min - 75' max
Lot Coverage <sup>9</sup>	75% max	75% max	90% max

#### HEIGHT<sup>10</sup>

Building Maximum	3.0 stories
------------------	-------------

### Notes

- All setbacks must be landscaped with a natural ground cover, excluding any alleys, pedestrian walkways, or patios. Maximum setback requirements are not applicable to garages.
- Greenways, when adjacent to the property line, shall be used as the front yard when determining setbacks. Covenants shall be submitted with approvals to ensure greenways are cared for.
- May be reduced to 0' on one side when adjacent lots are approved under one development plan to ensure separation between buildings on different properties.
- Attached and detached garages should take access off rear alleys when alleys are provided. Garages shall not occupy more than 24' of street frontages. (a standard two stall garage).
- Rear yard setback may be reduced to 0' when an alley is present or when adjacent lots are approved under one development plan to ensure separation between buildings on different properties.
- All buildings must have a primary ground-floor entrance that faces a primary or side street.
- Maximum lot width on corner lots is 100'.
- Maximum of 10 attached units in a row.
- Lot coverage includes impervious areas, defined as driveways, sidewalks, buildings, and patios.
- All heights measure to eaves or top of parapet.

## A2.3 - URBAN RESIDENTIAL (UR) STANDARDS, CONT.

### BUILDING FACADES

#### Materials

Facing ROW or Open Space	75% Primary Materials
Not facing ROW	50% Primary Materials
Materials are identified in the Design Guidelines Manual	

#### Design and Architecture<sup>1</sup>

Allowed techniques are identified in the Design Guidelines Manual.

### PARKING

#### Surface Parking<sup>2</sup>

Location	Single-Family	2-4 plex	Townhome
Yard	N/A	Rear	Rear

1. Surface parking includes areas other than driveways leading to a garage.

#### Required Spaces

Single-Family	1.0 space/Dwelling Unit Minimum
2-4 plex	1.0 space/Dwelling Unit Minimum
Townhome	1.0 space/Dwelling Unit Minimum

Access	Single-Family	2-4 plex	Townhome
	Front, Rear Alley, Side Street	Rear Alley	Rear Alley

#### Access Design

Access shall be hard surfaced and lead directly to a garage. Pavement should not be more than 2' wider than the garage. A minimum 3' pervious strip should be located between driveways of attached units to separate garages and prevent large expanses of impervious surface.

### ALLOWED SIGNAGE<sup>3</sup>

Sign Types	Height	Area	Number
Wall	< building	4 sf	1 per building
Temporary	Refer to section 176.31, Marion Zoning Ordinance		

All other signage standards shall be in accordance with section 176.31 under the City of Marion Zoning Ordinance.

### GROUND FLOOR FRONTAGE TYPES ALLOWED

General yard	Natural ground cover
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Stoops <sup>4</sup>	Depth 4' min
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Porches <sup>5</sup>	Depth 8' min. Height 1 story max.
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#### Notes

1. Mansard roof forms are not allowed.

2. Surface parking includes areas other than driveways leading to a garage.

3. Illuminated signs are not allowed.

4. Frontage where the building facade is aligned close to the front setback line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing

5. A planted frontage wherein the building facade is set back from the front setback line with an attached porch. A fence may be allowed at the front setback line to maintain the street spatial definition.



## A2.3 - URBAN RESIDENTIAL (UR) STANDARDS, CONT.

**TABLE A2.3: Urban Residential (UR) Permitted and Conditional Land Uses**

### LAND USE TYPE <sup>1</sup>

#### Residential

Artist Live/Work Space	P
Bed & Breakfast	C
Day Care Home	C
Dwelling Unit, Auxiliary	A
Dwelling, Efficiency/Micro Units	P
Dwelling, One-Unit <sup>2</sup>	P
Dwelling, Two-Unit <sup>2</sup>	P
Dwelling, Three/Four-Unit <sup>2</sup>	P
Group Home	C
Home Occupation	P

#### Cultural, Religious, Recreation & Entertainment

Outdoor Entertainment, Temporary	C
Park, Neighborhood	P
Place of Worship	P
Public Plaza	P

#### Governmental and Institutional

Library	P
Office, Government	P
Post Office	P
Public Service Facility	C
School, Specialized Instruction	P

#### Other

Utility, Community/Regional	C
Utility, Local	C

#### Key

- P = Permitted Use
- C = Conditional Use
- A = Permitted as Accessory Use

#### Notes:

1. A definition of each listed use type is in Section 176.06 of the Marion Zoning Regulations.

## A2.4 - SUPPLEMENTAL STANDARDS AND CONDITIONS

### GAS STATIONS IN THE NEIGHBORHOOD AT INDIAN CREEK

Gas stations are allowed as a conditional use within the Neighborhood Mixed Use (MX) district subject to the following standards to promote quality design and reduce the impacts of vehicle circulation, parking, lighting, and other activities associated with gas stations on adjacent properties. All other standards of the zoning ordinance and MX district apply if not specifically listed below.

#### Location and Site Design

1. Pumps and canopies shall be located either:
  - a. Behind the principal building, not visible from the street or public space; or
  - b. On corner lots may be located on the side of the principal building. However, corner side pumps and canopies shall not be oriented toward 35th Street, 35th Avenue, Tower Terrace Road, or public parks, schools, and open spaces.
2. On corner lots, the principal building shall be built to the intersection.
3. The principal building shall be built to the property line along a minimum of 60% of the primary street facing frontage(s). No street shall be entirely fronted with pumping stations and parking.
4. A maximum of four pumps are allowed per gas station with no more than eight vehicles to be fueled at one time. A pump is defined as a single pumping fixture that may have fuel pumping nozzles on both sides.
5. Driveways, stacking lanes, and pumping areas shall be buffered from public streets, sidewalks, and any residential use including mixed-use buildings by a 10 foot planting strip, with shrubs and/or trees that form a barrier at least 30 inch tall. A decorative fence or other non-natural barrier can be used with fewer plantings to form an opaque barrier. Other screening requirements in the code for trash and mechanical equipment shall be met.
6. Lighting shall be shielded downward to prevent glare onto adjacent properties.

#### Circulation and Parking

1. Pedestrian routes measuring at least five feet wide shall be defined and unobstructed, differentiated from vehicle areas, to the building entrance from the public sidewalk and pumping areas.
2. A maximum of two vehicle access points are allowed and can be from either the primary or secondary facing street.
3. The minimum number of parking spaces shall be 3.5 spaces per 1,000 gross square feet of building area.
4. Electric charging stations count toward minimum parking requirements.

#### Architectural and Canopy Standards

1. Buildings are subject to the architectural standards of the MX District.
  - a. Canopies shall have similar color and architectural features as the primary building. The form, pitch, colors, materials, and architectural details used for the canopy shall be designed to appear as an extension of the roof covering the principal building.
  - b. The colors, materials, and architectural details of the canopy supports shall be covered with the primary material used on the street facing façade of the principal building, as listed under the architectural and building design standards.
  - c. Signage is not allowed on the canopy or canopy supports.
  - d. The maximum length and width of a canopy structure is 100 feet.
  - e. Canopies must be located on the same lot as the principal building.

#### Miscellaneous Standards

1. No outdoor storage or automotive repairs are allowed on site.
2. Accessory car washes are allowed.
3. Temporary and portable signs are not allowed anywhere on site.
4. One freestanding monument sign is allowed.
  - a. Maximum sign area is 36 square feet
  - b. Maximum height is 8 feet
  - c. Other design standards of the MX District apply.

## APPENDIX 3: ARCHITECTURAL STANDARDS

### A3.1 - ARCHITECTURAL MATERIAL LIST

The following tables show permitted materials for exterior building finishes in The Neighborhood at Indian Creek. These material standards shall be used in conjunction with the architectural material standards described in this design manual.

**TABLE A3.1:** District Material List, Primary Materials

PERMITTED MATERIALS P = Permitted	UR <sup>1</sup>	MUR <sup>1</sup>	MX <sup>1</sup>
Fired Clay Brick, full veneer masonry wall system	P	P	P
Natural Stone, full veneer masonry wall system	P	P	P
Integrally colored, specialty concrete block such as textured, burnished, concrete masonry units (CMU)	X	P	P
Limestone - White, light buff, and course yellow	P	P	P
Wood - (painted or stained)	P	X	X
Wood - Cementitious wood siding	P	X	X
Non-tinted Glass	X	P	P
Cast Stone	X	P	P
FRC (Fiber Reinforced Concrete)	P	P	P
Terra Cotta	X	P	P
Metal wall panel systems, insulated or rain screen assemblies	X	P	P
Glazing (glass)	X	X	X
Pre Cast	X	X	X

**TABLE A3.2:** District Material List, Secondary Materials

PERMITTED MATERIALS P = Permitted	UR <sup>1</sup>	MUR <sup>1</sup>	MX <sup>1</sup>
Secondary Materials:	X	X	X
Copper or Zinc metal panels	X	P	P
Fiber Cement wall panel systems, insulated or rain screen assemblies	X	P	P
Standing Seam	X	P	P
Thin Veneer Brick or Tile (Adhered)	X	X	X
Manufactured or cast stone veneers (adhered)	X	X	X
Water-managed exterior Insulation and finish System (EIFS)	X	P	P
Fiber-cement siding	P	P	P
Cast in Place concrete	X	X	X
Weathering steel (Corten)	X	X	X
Any material in the Primary Material list above that does not count (x) as a Primary Material	P	P	P

**TABLE A3.3:** District Material Percentage Application

PERCENTAGE APPLICATION	UR <sup>1</sup>	MUR <sup>1</sup>	MX <sup>1</sup>
<b>Primary Material (%)</b>			
Street and Public Facing Facades	75	75	75
Non-Street and Public Facing Facades	50	50	50
<b>Secondary Material (%)</b>			
Street and Public Facing Facades	25	25	25
Non-Street and Public Facing Facades	50	50	50
<b>Not permitted (X)</b>	0	0	0

**TABLE A3.4:** District Material List, Special Exception Necessary (limited to percentage indicated)

MATERIAL	UR <sup>1</sup>	MUR <sup>1</sup>	MX <sup>1</sup>
Stucco, traditionally applied	<25	<25	<25
Tilt-up concrete wall panel system	<25	<25	<25
Glass Block	<25	<25	<25
Corrugated or ribbed metal panel	<25	<25	<25
Decorative asphalt, wood, or synthetic shingles	<25	<25	<25
Textured architectural concrete panels	<25	<25	<25
Any material listed not permitted (X)	Up to allowed district percentage for primary or secondary material		
Any material not listed on this table <sup>2</sup>	<25	<25	<25

**TABLE A3.5:** District Material List, Not Permitted in Any District

MATERIAL	UR <sup>1</sup>	MUR <sup>1</sup>	MX <sup>1</sup>
Plywood	X	X	X
Painted brick <sup>3</sup>	X	X	X
Plastic/vinyl siding	X	X	X
Smooth faced concrete masonry units (CMU)	X	X	X

**TABLE NOTES**

1. Material standards apply to all uses in The Neighborhood Planned Development
2. Any materials similar to those under the primary and secondary category may be approved at the respective allowed percentage if demonstrated that they have comparable durability and impact resistance (based on manufacturer's specifications), aesthetic quality, and neighborhood context as the materials permitted above
3. Brick and stone should be colored only by means of pigment impregnation throughout the entire material and not be painted only on the exterior surface.

## A3.2 - BUILDING VARIETY

All principal buildings in The Neighborhood shall be of high quality design and construction as described throughout the Design Guidelines. While individual building design and material selection are critical to the character of The Neighborhood, the collection of building styles and materials is equally important.

To prevent monotony in building types that detracts from the overall character of The Neighborhood, the following variation in building styles shall be applied.

### Applicability

Block faces with at least three (3) and less than ten (10) buildings shall have a minimum of two (2) different building types. Block faces with ten (10) or more buildings shall have a minimum ratio of one (1) different building type per every five (5) buildings (rounding up the nearest whole number of building types).

A building type is defined as having two (2) distinct changes in primary materials, colors, or architectural features on facades facing streets, alleys, greenways, or public spaces, when compared to other buildings on the block face. Building types shall apply to attached and detached residential structures.

Single townhome structures with more than six (6) units shall have two (2) distinct changes in primary materials, colors, or architectural features on facades facing streets, alleys, greenways, or public spaces, when compared to other units in the combined townhome structure.

### Definitions

A distinct change in material means:

1. A different permitted primary material from Table A3.1.
2. Secondary and special exception materials are allowed to be the same on all buildings.

A distinct change in color means:

1. A different distinctive hue of natural earth tone exterior building color.
2. Secondary and special exception materials are allowed to be the same color on all buildings.
3. A change in building material can also constitute a change in building color depending on the material color. For example, red fired clay brick is a different color from permitted limestone (white, light buff, coarse yellow).

Architectural features in the context of building variety means building height, building frontage, and building style. A distinct change in architectural features means:

1. Height: A change in building height by at least one story.
2. Frontage: Distinct differences in allowed ground floor frontage types (porches and stoops).

3. Style: Distinct architectural styles as defined by their common architectural design. Examples include Victorian, Prairie, Arts and Crafts, Tudor, Four Square and Colonial.

### Exemptions

The following are exempt from the building variety standards:

1. Buildings in the MX district.
2. Facades not facing a street, alley, greenway, or public space.
3. Accessory buildings.

### Examples:

