



East Marion Subarea Plan

Plan Summary
October 11, 2021



Site Analysis

East Marion Subarea Plan | Site Analysis

- Drainage Assumptions

- No hydrology study completed
- Stormwater assumptions are planning level

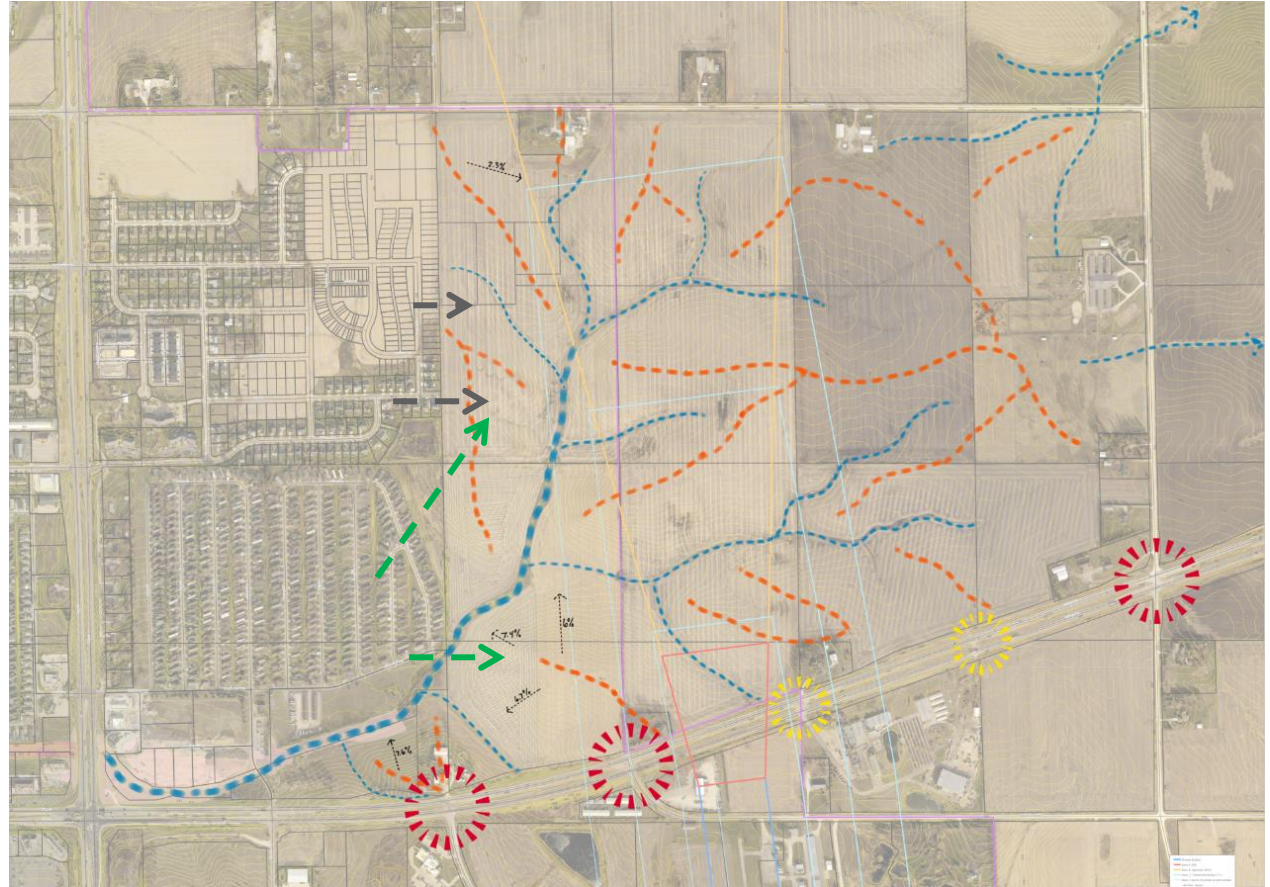
- Slopes / Topography

- Current drainage paths in blue; ridges in orange

- Highway Access

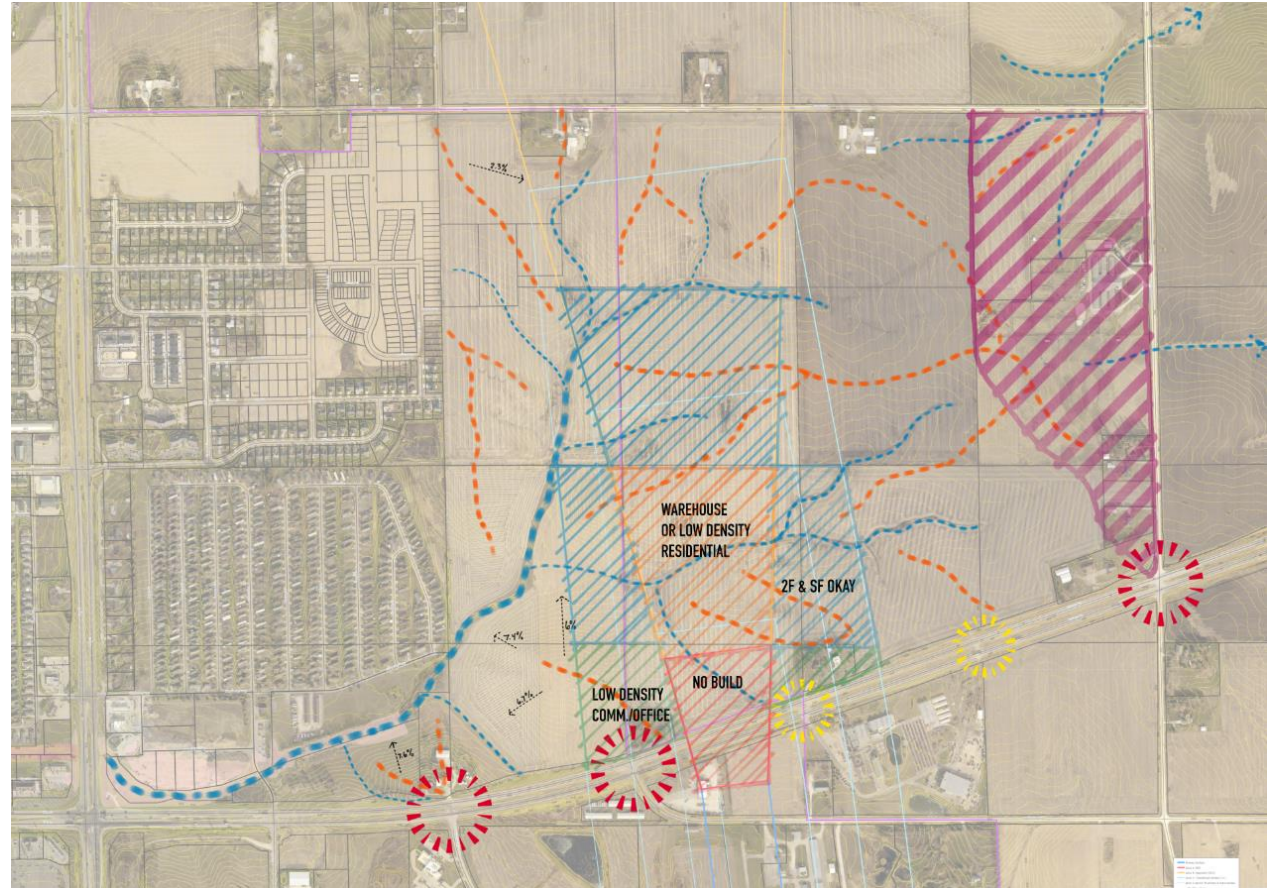
- Planned full access/signalized intersections shown in red; limited and/or unsignalized access shown in yellow

- Vehicular/ Pedestrian Connectivity



East Marion Subarea Plan | Site Analysis

- Drainage Assumptions
- Slopes / Topography
- Highway Access
- Sewer
 - It is anticipated that sewer service in hatched purple area will be provided later than the rest of the subarea
- Airport Restrictions





Concept Review

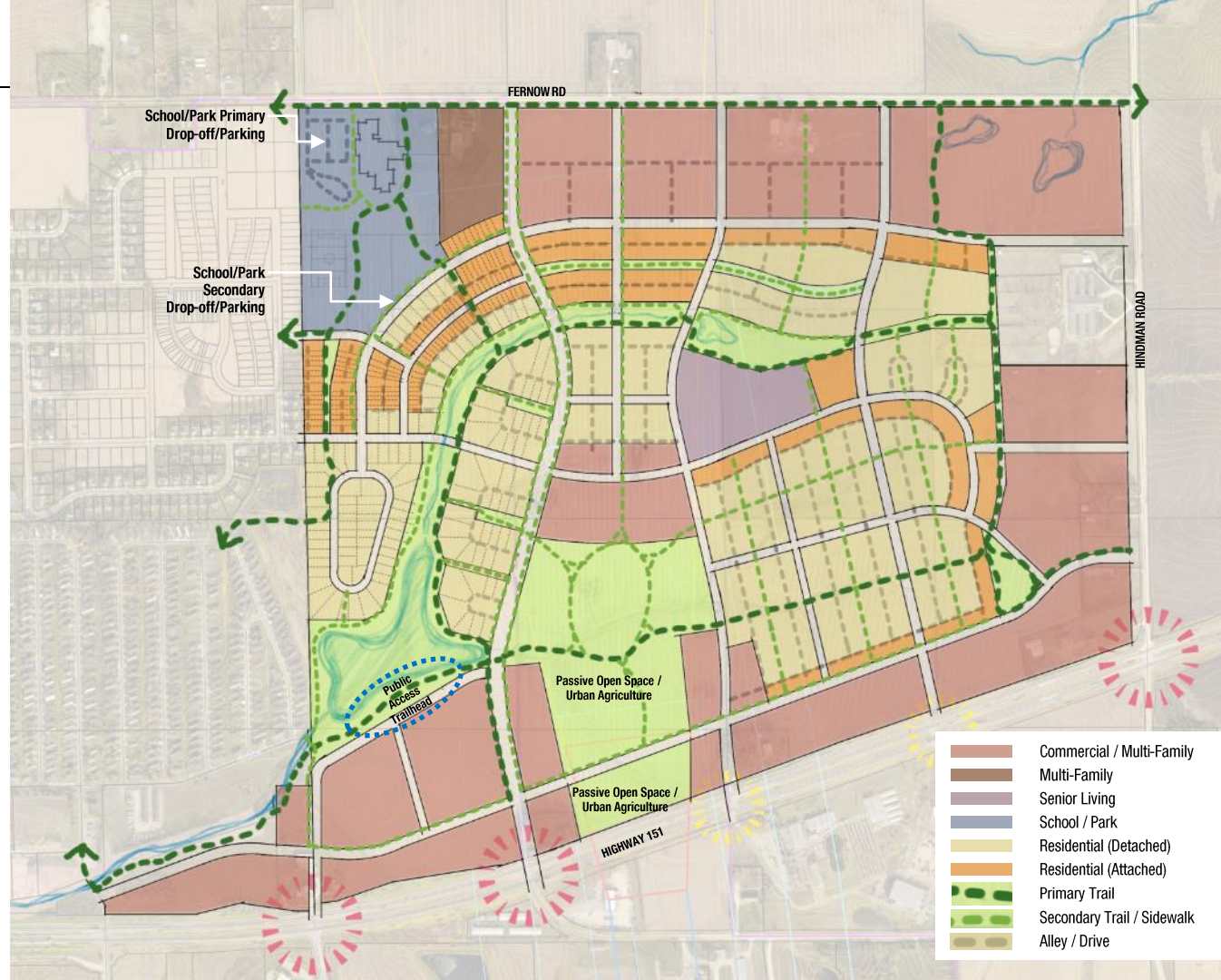
East Marion Subarea Plan | **Planning Principles**

- Walkable & connected
- Work with the constraints to create a meaningful neighborhood
- Incorporate a variety of neighborhood & community scale land uses integrated horizontally
- Access to open space is balanced with development feasibility
 - Pedestrian Access / Greenway & Pocket Neighborhoods
 - Vehicular Access - Public Streets & Public Alleys
- Adjust north-south collector to adapt to the land and provide opportunity for neighborhood creation.



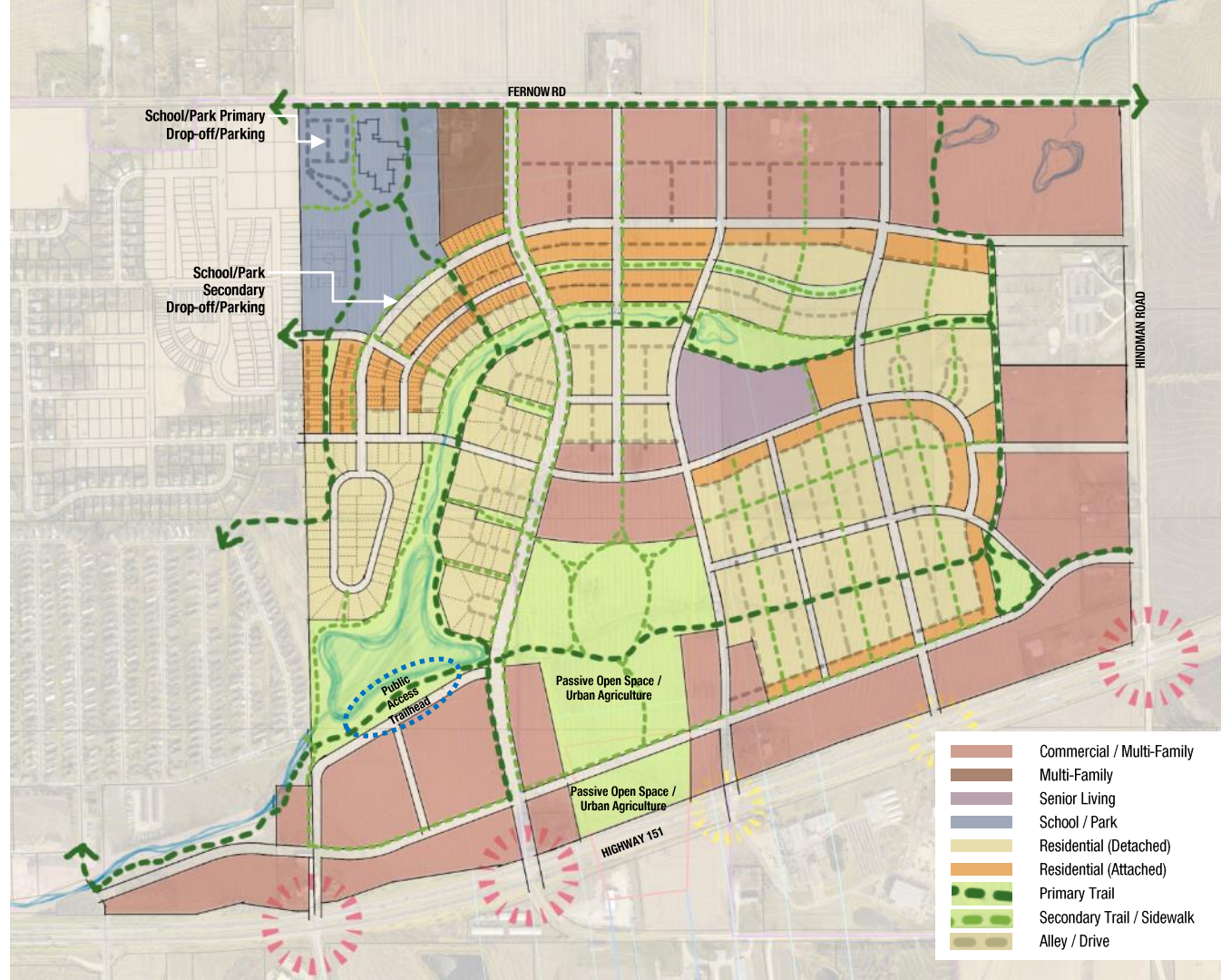
Plan Concept

- Walkable & connected
- Work with the constraints to create a meaningful neighborhood
- Variety of neighborhood & community scale land uses integrated horizontally
- Access to open space is balanced with development feasibility
 - Pedestrian Access / Greenway & Pocket Neighborhoods
 - Vehicular Access
- North-South Collector Adjustment

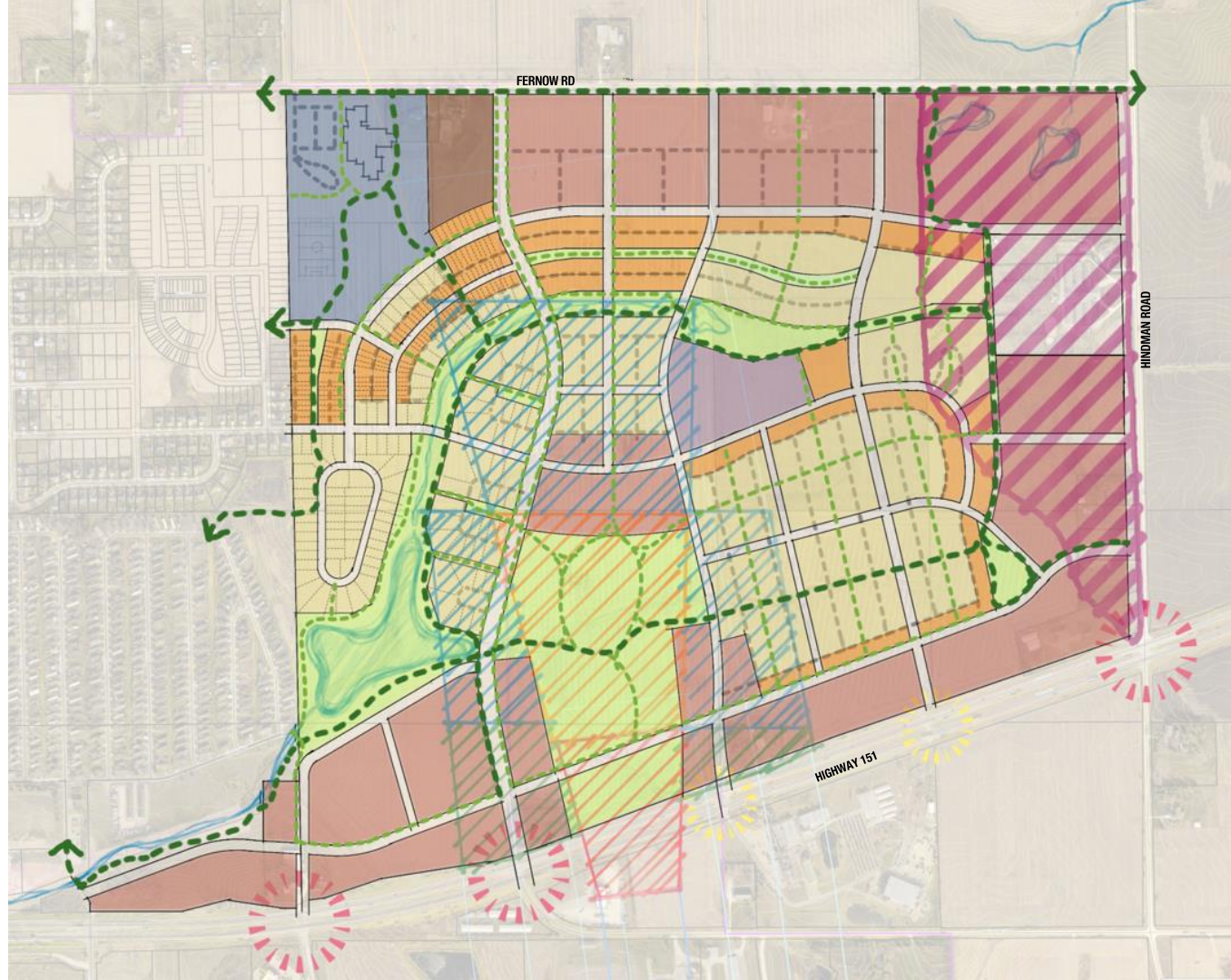


Commercial / Multi-family

- Commercial & multi-family uses along Fernow Road are envisioned to be primarily multi-family with the potential for neighborhood-scale commercial uses.
- Commercial & multi-family uses along Highway 151 are envisioned to be primarily commercial.



Plan Concept – Airport /Sewer Overlay



Land Use Estimate

Pelley Property + Collector Adjustment

School / Park - 26.2 acres

Commercial / Multi-Family – 31.2 acres

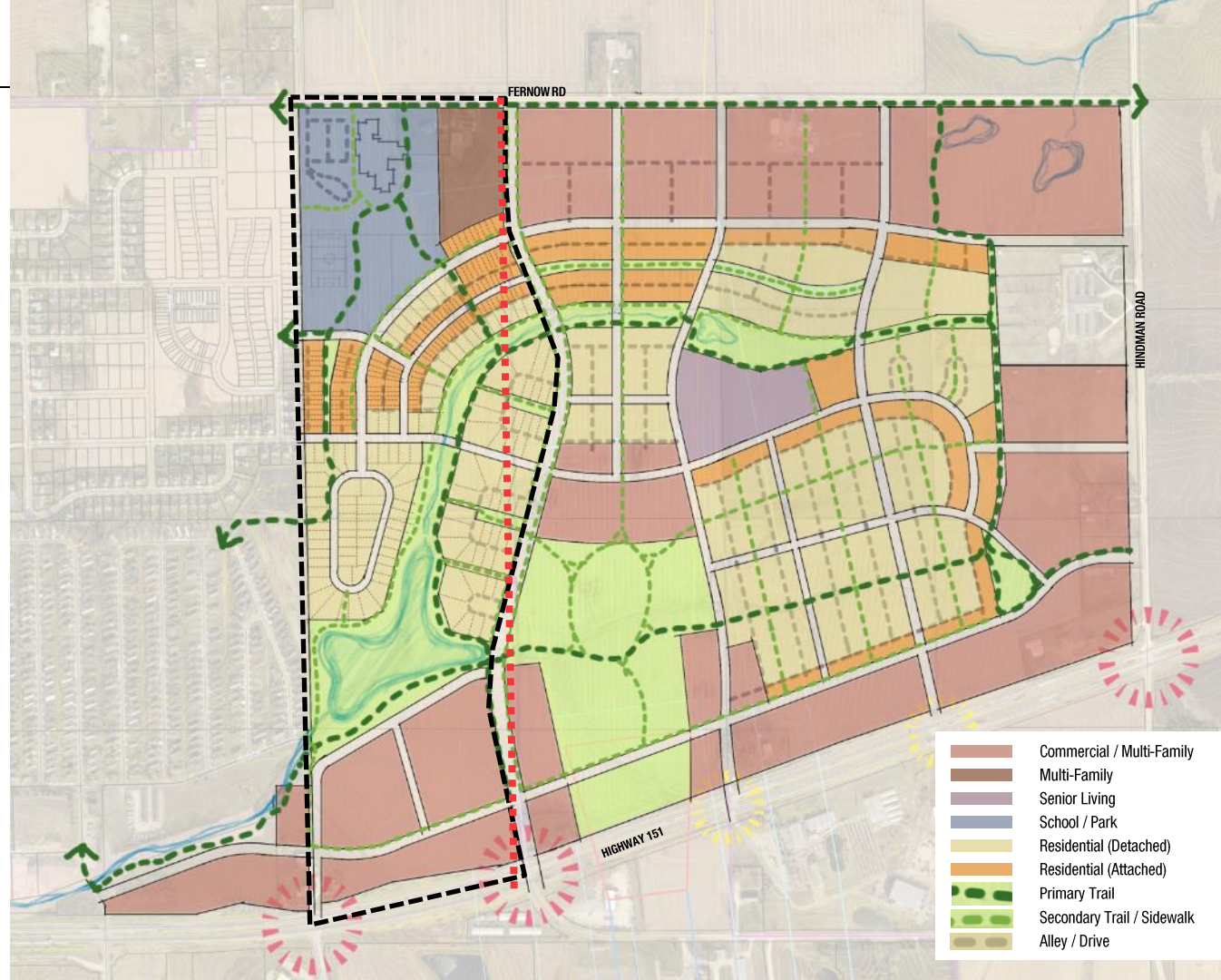
- 32.9 acres, 4/1/2020 plan (included part of Esker property)

Rowhomes / Duplex - 147 Units

- 142 units, 4/1/2020 plan

Single Family Units – 134 units

- 154 units, 4/1/2020 plan





Character Imagery

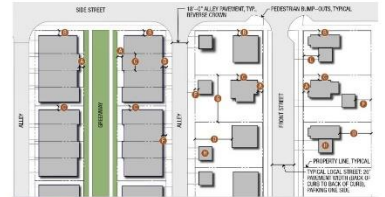
Greenways & Pocket Parks Character Imagery



The Neighborhood at Indian Creek - Single-Family Homes

A2.3 - URBAN RESIDENTIAL (UR) STANDARDS

The UR district encompasses the areas within the Neighborhood at Indian Creek Master Plan designed as low to medium density residential in the land use plan. The UR district is an area where a mix of low to medium density residential uses are encouraged such as single-family dwellings, duplexes, and townhomes. The intent is to realize a traditional neighborhood development complemented by community commercial and civic uses. The style of the residential uses should provide a cohesive look and feel to the district.



BUILDING PLACEMENT: Principal Building Setback (Distance from Property Line or alley easement)

- 1. Single-Family 2-4 story Townhome
- 2. Front: 8' min - 15' max
- 3. Side: 5' min - 15' max
- 4. Rear: 5' min

Accessory Building and Attached Garage Setback

- 1. Front/Side Setback: 20' in front of garage doors, min.
- 2. All other: 5' more than primary building setback
- 3. Rear Setback: 5' min

Building Form

- 1. Single-Family 2-4 story Townhome
- 2. Front Setback: 30' min - 35' min
- 3. Lot Width: 85' max 140' max 75' min
- 4. Lot Coverage: 75% max 75% max 90% max

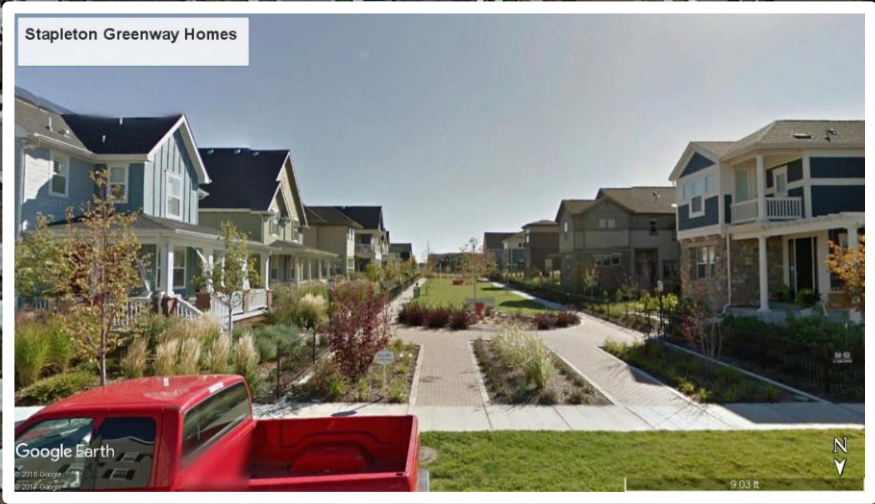
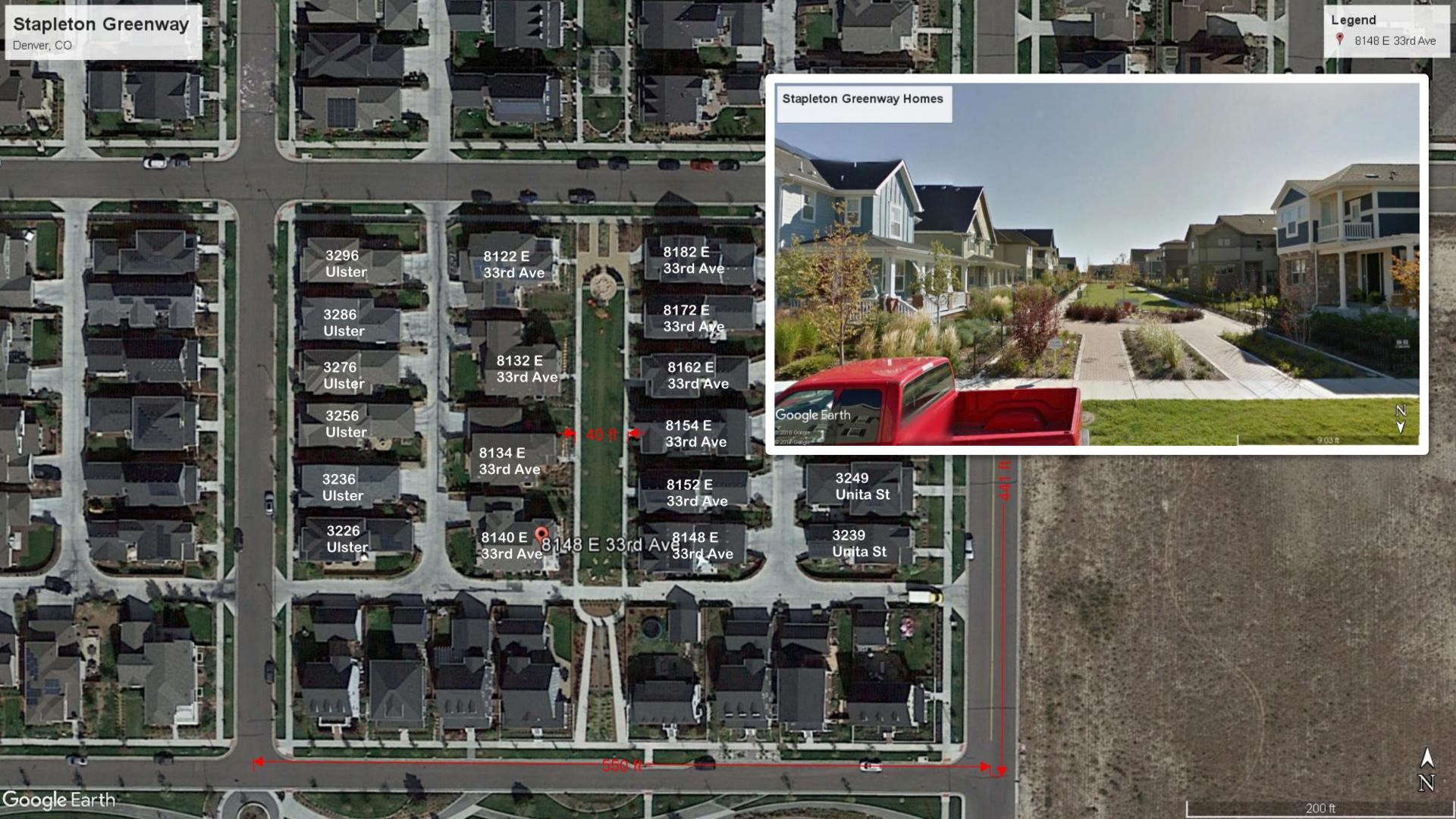
HEIGHT

- 1. Building Maximum: 3.0 stories

1. All setbacks must be landscaped with a natural ground cover, including any alleys, easement walkways, or public. Maximum setback requirements are not applicable to garages.
2. Greenways, when adjacent to the property line, shall be used as the front and/or side setback. Greenways shall be subject to the same setbacks as the adjacent lot. Greenways shall be subject to the same setbacks as the adjacent lot.
3. May be reduced to 7' on one side when adjacent into the agreement under development plan to ensure separation between buildings on different properties.
4. Attached and detached garages should be located off rear alley, other when no parking, or on corner. Setback shall not exceed more than 24' of street the height. (is standard low ball garage)
5. Rear yard setbacks may be reduced to 0' when an alley is present or other adjacent lots are approved under one development plan to ensure separation between buildings on different properties.
6. All buildings must have a primary ground-floor entrance that faces a primary or side street.
7. Maximum lot width on corner lots is 100'.
8. Maximum of 10 attached units in a row.
9. Lot coverage includes impervious areas, defined as driveways, sidewalks, walkways, and patios.
10. All heights measure to eaves or top of parapet.

Gray's Station - Des Moines, IA





3296
Ulster

8122 E
33rd Ave

8182 E
33rd Ave

3286
Ulster

8132 E
33rd Ave

8172 E
33rd Ave

3276
Ulster

8162 E
33rd Ave

3256
Ulster

8134 E
33rd Ave

8154 E
33rd Ave

3236
Ulster

8152 E
33rd Ave

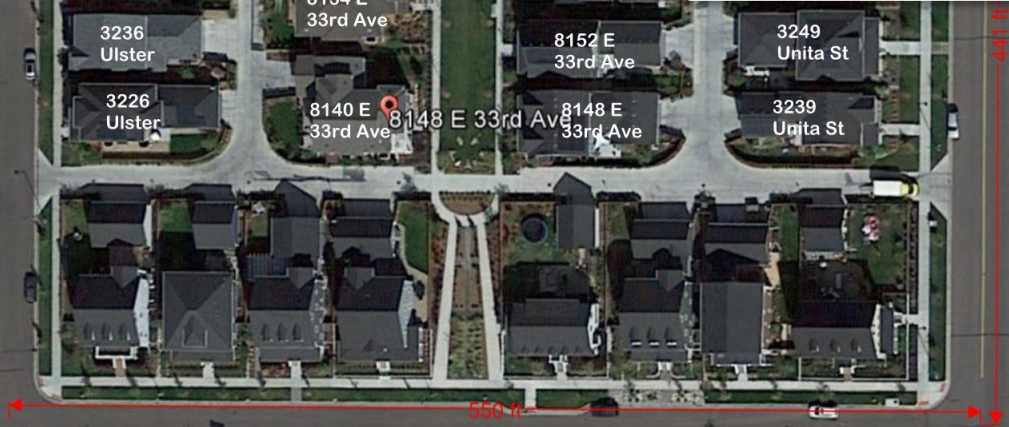
3226
Ulster

8140 E
33rd Ave

8148 E
33rd Ave

3249
Unita St

3239
Unita St



Semi-Attached Rowhomes – Omaha



